DESERT HAVEN (QUEENS MOTEL) RENOVATION PROJECT

16959 STODDARD WELLS ROAD VICTORVILLE, CA 92395

APN: 0472-181-68

A.C.	AIR CONDITIONING
ACOUS.	ACOUSTICAL
ADJ.	ADJUSTABLE
A.F.F.	ABOVE FINISH FLOOR
ALUM.	ALUMINUM
ANOD.	ANODIZED
BD.	BOARD
BLDG.	BUILDING
BLKG.	BLOCKING
BOT.	BOTTOM
BM.	BEAM
CAB.	CABINET
CEM.	CEMENT
CER.	CERAMIC
C.G.	CORNER GUARD
C.L.	CENTER LINE
CLG.	CEILING
CLR.	CLEARANCE
CMU.	CONCRETE MASONRY UNIT
COL.	COLUMN
CONC.	CONCRETE
CONST.	CONSTRUCTION
CONTR.	CONTRACTOR
C.T.	CERAMIC TILE
CTSK.	COUNTERSINK
CTR.	CENTER
DA	DISABLED ACCESSIBLE
DBL.	DOUBLE
D.F.	DOUGLAS FIR
DET.	DETAIL
DIAG.	DIAGONAL
DIA.	DIAMETER
DIM.	DIMENSION
DN.	DOWN
DR.	DOOR
DWG.	DRAWING
D.S.	DOWNSPOUT
(E)	EXISTING
EA.	EACH
ELEC.	ELECTRICAL
E.P.	ELECTRICAL PANEL
ELEV.	BUILDING ELEVATION
EQ.	EQUAL
EXP.	EXPOSED
EXT.	EXTERIOR
F.FLR.	FINISH FLOOR
FLASH	FLASHING
F.O.F.	FACE OF FINISH
F.O.S.	FACE OF STUD
FR.C.	FRENCH CASEMENT
FTG.	FOOTING
FX.	FIXED
GA.	GAUGE
GALV.	GALVANIZED
GL.	GLASS
GR.	GRADE
GWB.	GYPSUM WALL BOARD
GYP.	GYPSUM
H.A.R.	HOT AIR REGISTER
HDR.	HEADER
HGR.	HANGER
HDWD.	HARDWOOD
HORZ.	HORIZONTAL
HT.	HEIGHT
HVAC.	HEATING, VENTILATING
INT.	INTERIOR
INST.	INSTALLATION
INSUL.	INSULATION
LAM.	LAMINATE
LAV.	LAVATORY
MAT.	MATERIAL
MAX.	MAXIMUM
M.B.	MACHINE BOLT
M.C.	MEDICINE CABINET
(M)	MODIFIED
MIN.	MINIMUM
MIR.	MIRROR
MISC.	MISCELLANEOUS
MTL.	METAL
MTD.	MOUNTED
(N)	NEW
N.I.C.	NOT IN CONTRACT
NO.	NUMBER
NTE	NOT TO EXCEED
N.T.S.	NOT TO SCALE
O.C.	ON CENTER
O.D.	OUTSIDE DIAMETER
O.H.	OVER HEAD
PL.	PLATE
PLAM.	PLASTIC LAMINATE
PLYWD.	PLYWOOD
PSI	POUNDS/SQ. INCH
PT	PRESSURE TREATED
Q.T.	QUARRY TILE
R	RISER
(R)	REPLACE
REINF.	REINFORCING
REQD	REQUIRED
REV.	REVISED
RM.	ROOM
RO.	ROUGH OPENING
RWL	RAIN WATER LEADER
SAD	SUPPLY AIR DUCT
SCHED.	SCHEDULE
SEC.	SECTION
SHT.	SHEET
S&P	SHELF AND POLE
SL	SKYLIGHT
SIM.	SIMILAR
SPEC.	SPECIFICATION
SQ.	SQUARE
S.S.	STAINLESS STEEL
S.S.D.	SEE STRUTURAL. DRAWINGS
S.M.D.	SEE MECH. DRAWINGS
STD.	STANDARD
STL.	STEEL
STOR.	STORAGE
STRUCT.	STRUCTURAL
SUSP.	SUSPENDED
T.B.	TOWEL BAR
T.O.S.	TOP OF SLAB
TGL.	TEMPERED GLASS
T&G	TONGUE AND GROOVE
TYP.	TYPICAL
U.B.C.	UNIFORM BUILDING CODE
U.O.N.	UNLESS OTHERWISE NOTED
V.G.	VERTICAL GRAIN
VERT.	VERTICAL
V.I.F.	VERIFY IN FIELD
W/	WITH
WC	WATER CLOSET
WD.	WOOD
W.P.	WATERPROOF

ABBREVIATIONS

1. ALL WORK SHALL CONFORM TO THE FOLLOWING BUILDING CODES AND CALIFORNIA STATE BUILDING CODE AMENDMENTS, AS WELL AS AND ALL OTHER VICTORVILLE MUNICIPAL CODES - BUILDING AND CONSTRUCTION, ADDITIONAL VICTORVILLE REGULATIONS, AND NFPA STANDARDS.

   2016 CALIFORNIA BUILDING CODE (CBC)
   2016 CALIFORNIA ELECTRICAL CODE (CEC)
   2016 CALIFORNIA PLUMBING CODE (CPC)
   2016 CALIFORNIA MECHANICAL CODE (CMC)
   2016 CALIFORNIA GREEN BUILDING STANDARDS CODE (CALGREEN)

2. THE CONTRACTOR SHALL VERIFY ALL DIMENSIONS, CONDITIONS & SHALL NOTIFY THE ARCHITECT OF ANY DIMENSIONS & CONDITIONS, WHICH DIFFER FROM THOSE SHOWN, BEFORE STARTING WORK. DIMENSIONS GIVEN AS (CLR.) ARE CODE REQUIRED & SHALL BE MAINTAINED.

3. ALL DIMENSIONS FOR NEW CONSTRUCTION ARE TO FACE OF STUD. ALL DIMENSIONS FROM EXISTING CONSTRUCTION ARE FROM FACE TO FINISH, UNLESS OTHER WISE NOTED (U.O.N.).

4. DO NOT SCALE DRAWINGS. WRITTEN DIMENSIONS TAKE PRECEDENCE OVER SCALED DIMENSIONS. DIMENSIONS FOR EXISTING CONDITIONS & ELEVATIONS MAY BE APPROXIMATE.

5. ALL INSTALLATIONS TO BE IN ACCORDANCE WITH BEST INDUSTRY STANDARDS & MANUFACTURER'S REQUIREMENTS, SEE ATTACHED SPECIFICATIONS.

6. ALL THE DRAWINGS SHOW REPRESENTATIVE & TYPICAL ATTACHMENTS, CONNECTIONS, FASTENINGS & ETC. SHALL BE PROPERLY SECURED IN CONFORMANCE WITH BEST PRACTICE.

7. THE PLANS MAY OR MAY NOT BE BASED ON A SURVEY & ARE INTENDED ONLY TO SHOW GENERAL LAYOUT OF PROPERTY & WORK LOCATIONS. CONDITIONS SHOWN APPROXIMATE.

8. ALL WORK TO BE PAINTED TO MATCH (E), UNLESS OTHERWISE NOTED.

9. ALL WORK SUBJECT TO ASSOCIATED PROJECT MANUAL SPECIFICATIONS AND INSTRUCTIONS BY ARCHITECT.

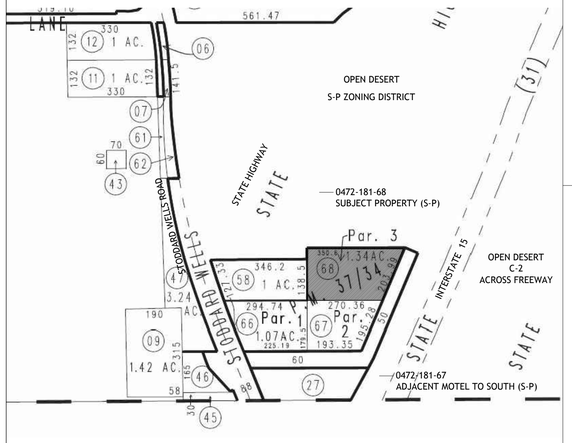
10. ALL CONDITIONS ARE EXISTING UNLESS OTHERWISE NOTED.

11. PROJECT IS "PUBLIC HOUSING" PER CBC 11B IS THE STANDARD FOR DESIGN. SOME 11 B ELEMENTS ARE TECHNICALLY INFEASIBLE DUE TO EXISTING CONDITIONS.

12. PROJECT CONVERTS OCCUPANCY FROM R1 TO R2

HAZARDOUS MATERIALS IN EXISTING CONSTRUCTION
BASIS ARCHITECTURE & CONSULTING ASSUMES NO RESPONSIBILITY FOR THE MANAGEMENT OF HAZARDOUS MATERIALS THAT MAY BE ON SITE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR INSURING THAT PERSONNEL WITHIN THE WORK AREA ARE PROTECTED FROM EXPOSURE TO HAZARDOUS MATERIALS. IF MATERIALS ARE DISCOVERED THAT MAY BE HAZARDOUS, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE OWNER AND CEASE WORK UNTIL CONDITIONS CAN BE MAINTAINED IN COMPLIANCE WITH ALL APPLICABLE REGULATIONS.

GENERAL NOTES, CODE INFO



ZONING PLAN

SITE

OVERLAY PAVING, ADD (N) PERIMETER FENCE, ROLLING VEHICLE GATE AND PEDESTRIAN GATE. FILL POOL AND DEVELOP SITE AMENITIES

BUILDING

CONVERT EXISTING MOTEL TO SUPPORTIVE HOUSING WITH COMMUNITY ROOM, MANAGER'S UNIT, 7 "EFFICIENCY" UNITS (ORIGINAL MOTEL ROOMS) AND 24 APARTMENTS CONVERTED FROM PAIRS OF MOTEL ROOMS, LAUNDRY, OFFICE AND MECHANICAL SPACES. ACCESSIBLE AND ADAPTABLE UNITS TO BE PROVIDED AT GROUND FLOOR.

RESTORE AND REFURNISH EXISTING BUILDING ELEMENTS TO REMAIN

SYSTEMS:

INSTALL (N) FIRE SPRINKLER AND LIFE SAFETY ALARMS W/ STROBE NOTIFIER AND CENTRAL NOTIFIER. UPDATE ELECTRICAL, PROVIDE INDIVIDUAL UNIT BREAKERS.

SCOPE OF WORK

SHEET INDEX

OCCUPANCY: R-2, B

BUILDING GROSS AREA: 16,140 S.F.
(NO CHANGES)

SEE A4.0 FOR UNIT AND AREA BREAKDOWN.

ZONING: SP (SPECIAL PROJECT)

TYPE V-B BUILDING, NO SPRINKLER

PROVIDED PARKING: 31 (INC. 3 ACCESSIBLE), OPEN

LOT COVERAGE:

LOT SIZE: 1.34 ACRES, 58,370 S.F.

LOT COVERAGE RATIO: 13.8%

LANDSCAPE AREA: 7,801 S.F.

LANDSCAPE COVERAGE AREA RATIO: .13%

ACCESSIBILITY NOTE:

PROJECT IS PUBLICLY FUNDED HOUSING AND IS SUBJECT TO CBC 11B REQUIREMENTS. 5% OF UNITS TO BE ACCESSIBLE, TWO ACCESSIBLE UNITS, BOTH TO BE LOCATED AT LOWER LEVEL. BALANCE OF LOWER LEVEL UNITS TO BE ADAPTABLE EXCEPT "EFFICIENCY" UNITS THAT ARE ORIGINAL MOTEL ROOMS, NOT ALTERED. ALL UPPER LEVEL UNITS TO BE "STANDARD".

Space	Area Each	Quantity	Sub-total
Ground Floor			
Managers Unit	692	1	692
Office Area	473	1	473
Efficiency Units A, B, C	221	3	663
In-Line Units 1, 4	442	2	884
Side by Side Units 2, 3, 5, 6,7,8, 9, 10	431	8	3448
Community Room	868	1	868
Boiler Room	268	1	268
Laundry Room	268	1	268
Covered Walkway Area	1021	1	1021
Ground Floor Gross			8585
Second Floor			
Efficiency Units D, E, F, G	216	4	864
In-Line Units 11, 12, 13, 16	431	4	1724
Side by Side Units 14, 15, 17, 18, 19, 20, 21, 22, 23, 24	430	10	4300
Storage and Mechanical	538	1	538
Covered Walkway Area	991	1	991
Second Floor Gross			8417
Total (Gross) Area			17002
Unit Totals			
Efficiency Units		7	
In-Line Units		6	
Side by Side Units		18	
Total Units		31	

PROJECT DATA

PHOTO - EXISTING CONDITIONS

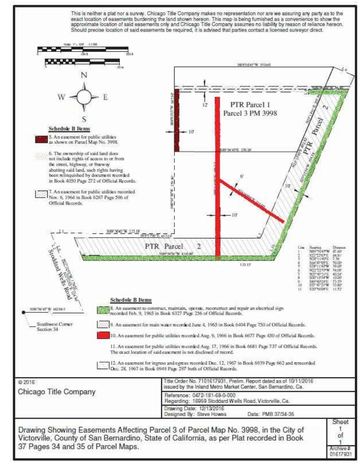
OWNER
DESERT HAVEN LP
715 E. BRIER DRIVE
SAN BERNARDINO, CA 92408
RON RUHL
909-332-6316
rruhl@hacsb.com

ARCHITECT
BASIS ARCHITECTURE & CONSULTING INC
2130 4TH STREET, STE. B
SAN RAFAEL, CA 94915
Architect: Charles Pick
415-457-6035
cpick@basisarch.com
Project Manager: Tessa Lombardi
tlombardi@basisarch.com

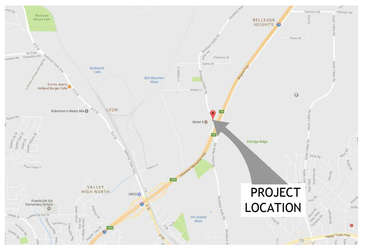
SITE DESIGN AND LANDSCAPE
BISNETT DESIGN
16046 BEAR COURT
GRASS VALLEY, CA 95949
BRIAN BISNETT
530-277-9733
brian@bisnettdesgin.com

MEP
AME DESIGN GROUP, INC.
2062 BUSINESS CENTER DRIVE, SUITE 250
IRVINE, CA 92612
GHASSAN SHREIM
949-553-0170
ghassan@amegroup.net

TEAM INDEX



EASEMENTS



VICINITY MAP

NORTH

BASIS Architecture & Consulting

2130 FOURTH ST
SAN RAFAEL, CA 94901
PHONE (415) 457-6035
FAX (415) 457-6036

P.O.BOX 150539
SAN RAFAEL, CA 94915

CHARLES PICK, ARCHITECT

LICENSED ARCHITECT
CHARLES PICK
C-21406
6-2019
RENEWAL DATE
STATE OF CALIFORNIA

DRAWING REVISION LOG

FOR BIO	5-15-2018

PROJECT NAME:

DESERT HAVEN
(QUEEN'S MOTEL)
RE-DEVELOPMENT

PROJECT LOCATION:

16959 STODDARD WELLS RI
VICTORVILLE, CA 92395

SHEET TITLE:

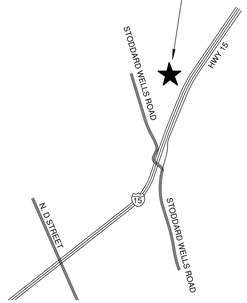
COVER SHEET

A0

LEGEND

- CONCRETE PAVING (NEW), SEE DETAIL-'C', L-7
- NEW PAVERS
- NEW PERMEABLE PAVERS
- COMPACTED DECOMPOSED GRANITE, SEE NOTES L-6
- RESURFACE PAVING: PATCH REPAIR POTHOLES, SLURRY SEAL AS SHOWN IN PARKING & DRIVE AREAS
- EXISTING ASPHALT PAVING TO REMAIN
- EXISTING ASPHALT PAVING TO BE REMOVED
- NEW FLUSH CURB, SEE DETAIL-'J', L-7
- NEW RAISED CURB, SEE DETAIL-'H', L-7
- NEW POLE LIGHT (7), SEE DETAIL-'G', L-8
- BENCH (6)
- PICNIC TABLE (8)
- ADA PICNIC TABLE (2)
- RAISED GARDEN PLANTERS (15) SEE DETAIL-'A', L-7
- 6' ORNAMENTAL IRON FENCE
- 16" SQ. SLUMP-STONE PILASTERS ALONG NEW FENCELINE @ 24' O.C. SEE DETAIL-'E', L-7
- ORNAMENTAL IRON FENCE w/2"x2" METAL POSTS @ 8' O.C., SEE DETAIL 'B', L-7
- SURE-LOCK STEEL EDGING HEADER, SEE DETAIL -'B', L-7
- ADA PATH OF TRAVEL
- NEW DROP INLET
- DS DOWNSPOUT
- PROPERTY LINE
- FENCE
- UTILITY VAULT
- ELEC BOX
- WATER VAULT
- SEWER VAULT
- GAS METER
- POWER POLE
- STREET LIGHT
- FIRE HYDRANT
- BACK FLOW PREVENTER
- MONUMENT AS DESCRIBED

DESERT HAVEN APARTMENTS
16959 STODDARD WELLS ROAD
VICTORVILLE, CA 92394

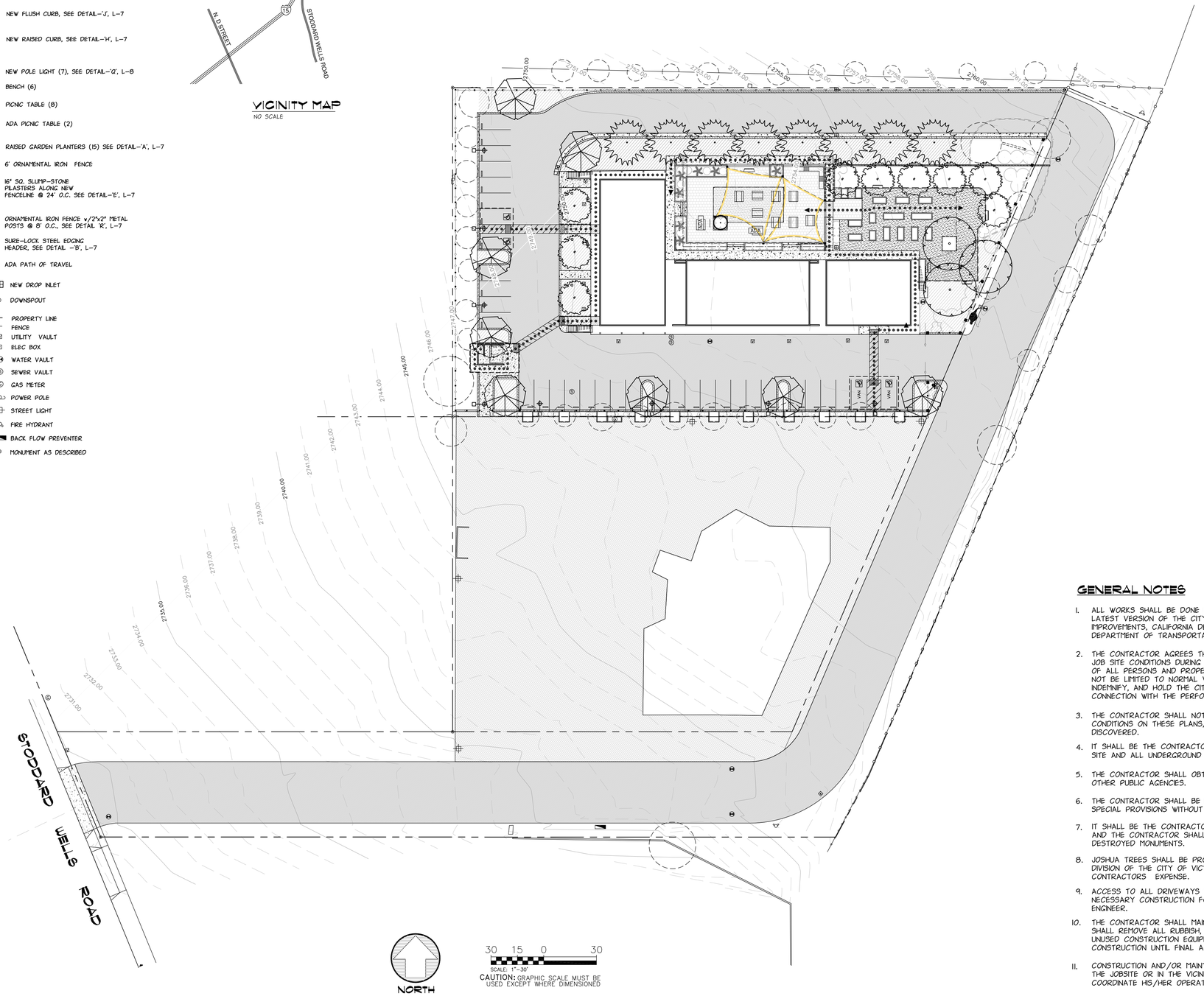


VICINITY MAP
NO SCALE

IMPROVEMENT PLANS FOR

DESERT HAVEN APARTMENTS

16959 STODDARD WELLS ROAD
VICTORVILLE, CALIFORNIA
APN 0472-181-68-0-000



NORTH

30 15 0 30
SCALE: 1"=30'
CAUTION: GRAPHIC SCALE MUST BE USED EXCEPT WHERE DIMENSIONED

DEVELOPMENT TEAM

APPLICANT / OWNER / DEVELOPER

JUDY DAVID
REAL ESTATE PROJECT MANAGER
HOUSING AUTHORITY OF THE COUNTY OF SAN BERNARDINO
715 E. BRIER DRIVE, SAN BERNARDINO, CA 92408
PHONE: (909) 332-6317 FAX (909) 890-4618

ARCHITECT

CHARLES PICK
BASIS ARCHITECTURE AND CONSULTING INC.
2130 4TH STREET, SUITE B
(MAIL: PO BOX 150539)
SAN RAFAEL, CA 94901
PHONE: (415) 578-4865

SITE PLANNING / DESIGN / CIVIL

BRIAN BISNETT
BISNETT DESIGN ASSOCIATES
16046 BEAR COURT
GRASS VALLEY, CA 95949
PHONE: (530) 277-9733

SHEET INDEX

PARCEL NOTES

APN	0472-181-68-0-000
FLOOD ZONE	'X' PER FEMA FIRM 06071C5820J, DATED 09-02-2016, FOR THE CITY OF VICTORVILLE, IN SAN BERNARDINO COUNTY, STATE OF CALIFORNIA,
PARCEL AREA	+/- 1.34 ACRES
EXISTING IMPERVIOUS AREA	+/- 1.14 ACRES
EXISTING PERVIOUS AREA	+/- .20 ACRES
PROPOSED IMPERVIOUS AREA	+/- .94 ACRES
PROPOSED PERVIOUS AREA	+/- .40 ACRES

(35) PARKING STALLS, INCLUDING (4) HANDICAP STALLS

(31) TOTAL UNITS... (24) 1 BEDROOM, (7) EFFICIENCY APARTMENTS

GENERAL NOTES

1. ALL WORKS SHALL BE DONE IN ACCORDANCE WITH THESE PLANS, SPECIAL PROVISIONS, AND LATEST VERSION OF THE CITY OF VICTORVILLE STANDARD SPECIFICATIONS FOR PUBLIC WORKS IMPROVEMENTS, CALIFORNIA DEPARTMENT OF TRANSPORTATION STANDARD PLANS, AND DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS.
2. THE CONTRACTOR AGREES THAT HE SHALL ASSUME SOLE AND COMPLETE RESPONSIBILITY FOR JOB SITE CONDITIONS DURING THE CORSE OF CONSTRUCTION OF THIS PROJECT, INCLUDING SAFETY OF ALL PERSONS AND PROPERTIES; THAT THIS REQUIREMENT SHALL APPLY CONTINUALLY, AND NOT BE LIMITED TO NORMAL WORKING HOURS; AND THAT THE CONTRACTOR SHALL DEFEND, INDEMNIFY, AND HOLD THE CITY HARMLESS FROM ANY AND ALL LIABILITY, REAL OR ALLEGED, IN CONNECTION WITH THE PERFORMANCE OF THIS PROJECT.
3. THE CONTRACTOR SHALL NOTIFY THE CITY ENGINEER OF ANY DISCREPANCIES OR UNUSUAL CONDITIONS ON THESE PLANS, THE SPECIAL PROVISIONS BEFORE BID AND AS SOON AS THEY ARE DISCOVERED.
4. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO FAMILIARIZE HIM/HERSELF WITH THE WORK SITE AND ALL UNDERGROUND UTILITIES/FACILITIES SHOWN OR NOT SHOWN ON THESE PLANS.
5. THE CONTRACTOR SHALL OBTAIN ALL PERMITS REQUIRED BY THE CITY OF VICTORVILLE AND OTHER PUBLIC AGENCIES.
6. THE CONTRACTOR SHALL BE HELD RESPONSIBLE ANY CHANGES MADE TO THESE PLANS AND THE SPECIAL PROVISIONS WITHOUT PRIOR WRITTEN AUTHORIZATION OF THE CITY ENGINEER.
7. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO PROTECT SURVEYING MONUMENTS IN PLACE, AND THE CONTRACTOR SHALL BE RESPONSIBLE FINANCIALLY FOR RESETTING DAMAGED OR DESTROYED MONUMENTS.
8. JOSHUA TREES SHALL BE PROTECTED IN PLACE OR RELOCATED AS APPROVED BY THE PARK DIVISION OF THE CITY OF VICTORVILLE, COMMUNITY SERVICES DEPARTMENT AT THE CONTRACTORS EXPENSE.
9. ACCESS TO ALL DRIVEWAYS SHALL BE MAINTAINED AT ALL TIMES EXCEPT WHEN PRECLUDED BY NECESSARY CONSTRUCTION FOR A REASONABLE PERIOD OF TIME AS APPROVED BY THE CITY ENGINEER.
10. THE CONTRACTOR SHALL MAINTAIN A NEAT APPEARANCE AT THE JOB SITE. THE CONTRACTOR SHALL REMOVE ALL RUBBISH, UNUSED MATERIALS, FORMS, BROKEN CONCRETE AND ASPHALT, UNUSED CONSTRUCTION EQUIPMENT, AND PLANTS AS SOON AS PRACTICABLE DURING CONSTRUCTION UNTIL FINAL ACCEPTANCE OF THE PROJECT BY THE CITY OF VICTORVILLE.
11. CONSTRUCTION AND/OR MAINTENANCE OPERATIONS BY OTHERS MAY OCCUR CONCURRENTLY AT THE JOBSITE OR IN THE VICINITY OF THE JOBSITE. THE CONTRACTOR SHALL COOPERATE AND COORDINATE HIS/HER OPERATION WITH THE OTHER CREWS.

NO.	DATE		BY
1	4-17-18	FOR REVIEW	
		ISSUED FOR	

NOTE
0 1/2 1
BAR SHOULD MEASURE 1" OR DRAWING IS NOT TO SCALE

SCALE:
DESIGNED:
DRAWN: JRS
CHECKED:
APPROVED:
LAST EDIT DATE:
CAD REF. FILE:

Bisnett Design Associates
bda
16046 Bear Court
Grass Valley, CA 95949
530.277.9733
brian@bisnettdesign.com
Landscape Architecture & Environmental Planning

DESERT HAVEN APARTMENTS
VICTORVILLE, CALIFORNIA

CIVIL COVER SHEET

6-11-18

DRAWING:
L-0

SURVEYOR'S STATEMENT

I. I FURTHER CERTIFY THAT THIS MAP OR PLAT AND THE SURVEY ON WHICH IT IS BASED WERE MADE IN ACCORDANCE WITH THE 2016 MINIMUM STANDARD DETAIL REQUIREMENTS FOR ALTA/NSPS LAND TITLE SURVEYS, JOINTLY ESTABLISHED AND ADOPTED BY ALTA AND NSPS, AND INCLUDES ITEMS 2, 3, 4, 6a, 6b, 7a, 7b I, 7c, 8, 9, 11, 13, 14, 16 AND 20 OF TABLE A THEREOF. THE FIELD WORK WAS COMPLETED ON 8-9-17.

2. THE ACCOMPANYING SURVEY WAS MADE ON THE GROUND AND CORRECTLY SHOWS THE LOCATION OF ALL BUILDINGS, STRUCTURES AND OTHER IMPROVEMENTS SITUATED ON THE PROPERTY DESCRIBED HEREIN AND THAT THERE ARE NO VISIBLE ENCROACHMENTS ON THE SUBJECT PROPERTY OR UPON ADJACENT LAND ABUTTING SAID PROPERTY, EXCEPT AS SHOWN HEREON.

3. THE PROPERTY DESCRIBED HEREIN IS THE SAME AS THE PROPERTY DESCRIBED IN CHICAGO TITLE COMPANY ORDER NO 7101620377, DATED MARCH 20, 2017, AND THAT ALL EASEMENTS, COVENANTS, AND RESTRICTIONS REFERENCED IN SAID TITLE COMMITMENT OR APPARENT FROM PHYSICAL INSPECTION OF THE SITE OR OTHERWISE KNOWN TO ME HAVE BEEN PLOTTED HEREON OR OTHERWISE NOTED AS TO THEIR EFFECT ON THE SUBJECT PROPERTY.

4. SAID PROPERTY IS LOCATED WITHIN AN AREA HAVING A ZONE DESIGNATION 'X' BY THE FEDERAL EMERGENCY MANAGEMENT AGENCY (FEMA), ON FLOOD INSURANCE RATE MAP NO. 06071C5820J, WITH A DATE OF IDENTIFICATION OF 09-02-2016, FOR THE CITY OF VICTORVILLE, IN SAN BERNARDINO COUNTY, STATE OF CALIFORNIA, WHICH IS THE CURRENT FLOOD INSURANCE RATE MAP FOR THE COMMUNITY IN WHICH SAID PROPERTY IS SITUATED.

5. THE PROPERTY DESCRIBED HEREIN HAS ACCESS VIA EASEMENT TO STODDARD WELLS ROAD, A DEDICATED PUBLIC STREET.

6. THE TOTAL NUMBER OF STRIPED PARKING SPACES ON THE PROPERTY DESCRIBED HEREIN IS 47, INCLUDING 1 DESIGNATED HANDICAP SPACE.

7. EXCEPT AS SHOWN HEREIN, ALL VISIBLE UTILITIES SERVING THE PROPERTY DESCRIBED HEREIN ENTER THROUGH ADJOINING PUBLIC STREETS AND/ OR EASEMENTS OF RECORD.

8. THERE ARE NO OBSERVABLE ABOVE GROUND ENCROACHMENTS (a) BY THE IMPROVEMENTS ON THE SUBJECT PROPERTY UPON ADJOINING PROPERTIES, STREETS OR ALLEY, OR (b) BY THE IMPROVEMENTS ON ADJOINING PROPERTIES, STREETS, OR ALLEYS UPON THE SUBJECT PROPERTY.

9. THERE IS NO OBSERVABLE EVIDENCE OF EARTH MOVING WORK, BUILDING CONSTRUCTION OR BUILDING ADDITIONS UPON THE SUBJECT PROPERTY.

10. ACCORDING TO LOCAL AGENCY THERE ARE NO PROPOSED CHANGES IN STREET RIGHT OF WAY LINES. THERE IS NO OBSERVABLE EVIDENCE OF RECENT STREET OR SIDEWALK CONSTRUCTION REPAIRS.

11. THERE IS NO OBSERVABLE EVIDENCE THAT THE SUBJECT PROPERTY IS OR HAS BEEN USED AS A SOLID WASTE DUMP, SUMP OR SANITARY LANDFILL.

12. THERE IS NO OBSERVABLE EVIDENCE THAT THERE ARE ANY CEMETERIES OR FAMILY BURIAL SITES ON SUBJECT PROPERTY.

13. THERE IS NO EVIDENCE OF RECORD THAT THERE ARE ANY GAPS OR GORES ON THE SUBJECT PROPERTY.

14. PROFESSIONAL LIABILITY INSURANCE POLICY OBTAINED BY THE SURVEYOR IN THE MINIMUM AMOUNT OF $1,000,000.00 IS IN EFFECT THROUGHOUT THE SURVEY TERM.

15. THERE IS NO OBSERVABLE EVIDENCE OF ANY CREEKS, STREAMS, RIVERS, LAKES, PONDS, WATERWAYS OR WETLANDS ON SUBJECT PROPERTY.

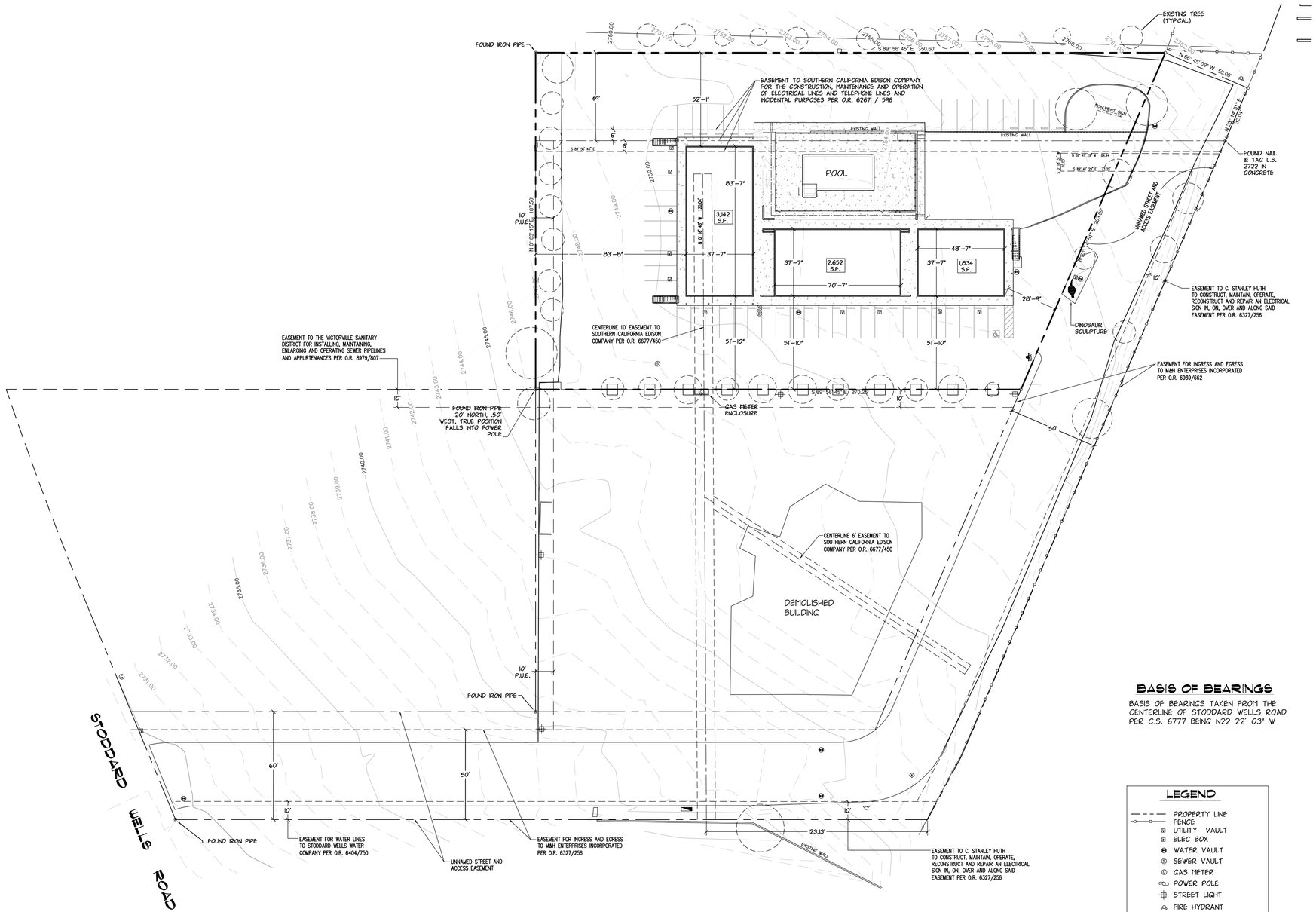
LICENSED LAND SURVEYOR
Stephen Hein
Ex. 9-30-18
No. 6792
STATE OF CALIFORNIA

SURVEYOR

REGISTRATION NO. _____

DATE: _____

JOB NO.
CALIFORNIA SURVEY CO.
136 IDAHO MARYLAND RD.
GRASS VALLEY, CA 95945
TELE (530) 273-6651



BASIS OF BEARINGS

BASIS OF BEARINGS TAKEN FROM THE CENTERLINE OF STODDARD WELLS ROAD PER C.S. 6777 BEING N22 22' 03" W

LEGEND

- PROPERTY LINE
- FENCE
- UTILITY VAULT
- ELEC BOX
- WATER VAULT
- SEWER VAULT
- GAS METER
- POWER POLE
- STREET LIGHT
- FIRE HYDRANT
- BACK FLOW PREVENTER
- MONUMENT AS DESCRIBED
- CONCRETE
- ASPHALT
- LANDSCAPE AREAS ARE UNHATCHED

NO.	DATE	ISSUED FOR	BY
1	4-17-18	FOR REVIEW	

NOTE
0 1/2 1
BAR SHOULD MEASURE 1" OR DRAWING IS NOT TO SCALE

SCALE:
DESIGNED:
DRAWN: JRS
CHECKED:
APPROVED:
LAST EDIT DATE:
CAD REF. FILE:

Bisnett Design Associates
bda
16646 Bear Court
Grass Valley, CA 95949
530.277.3725
brian@bisnettdesign.com
Landscape Architecture & Environmental Planning

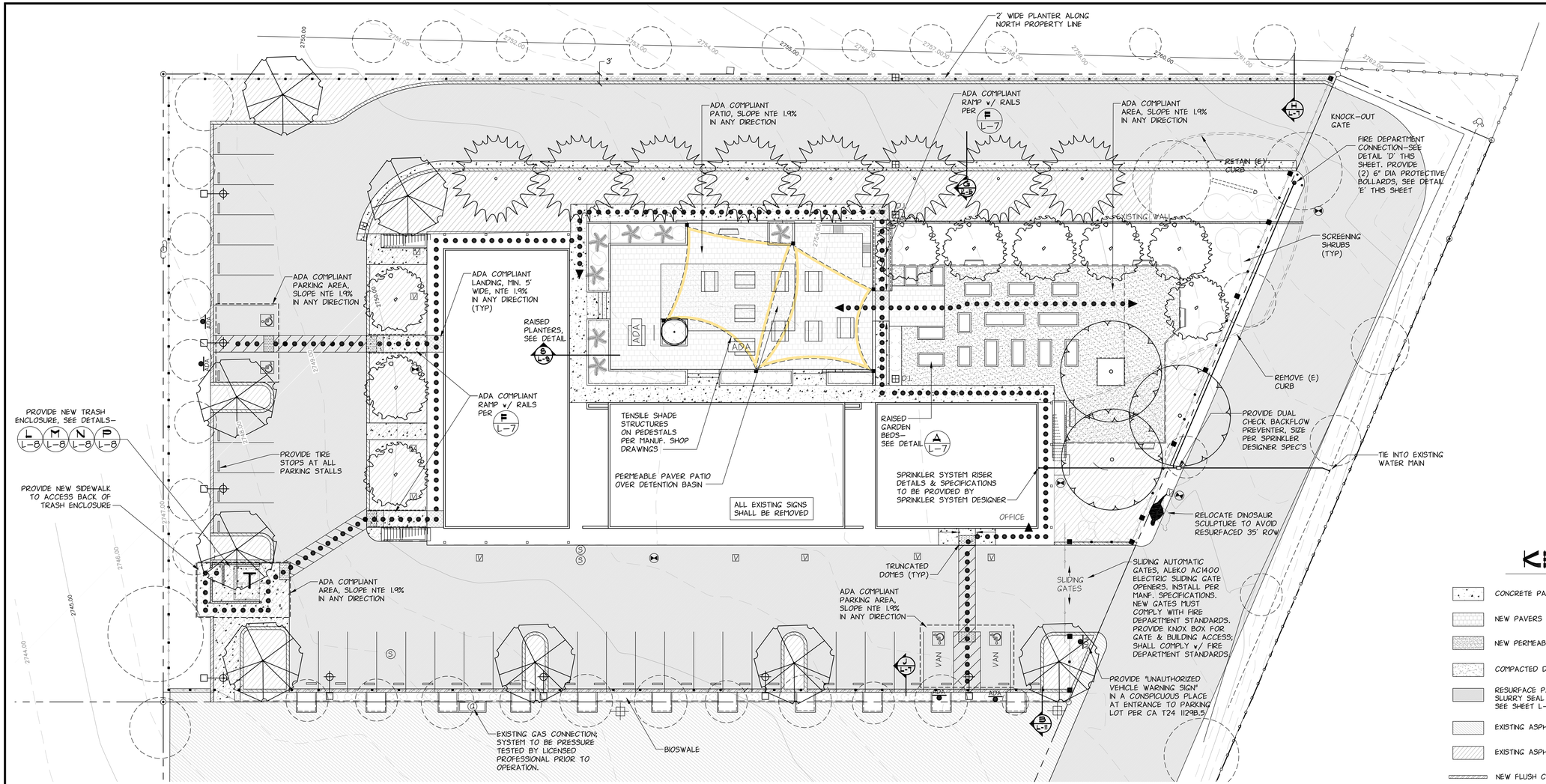
DESERT HAVEN APARTMENTS
VICTORVILLE, CALIFORNIA

TOPOGRAPHIC SURVEY

6-11-18

DRAWING::

L-1



AREAS

COMMUNITY SPACE	
PATIO	3,540 S.F.
GARDEN	3,272 S.F.
LANDSCAPE	13,802 S.F.
CIRCULATION	
(E) RESURFACED ASPHALT	22,426 S.F.
(E) CONCRETE SIDEWALK	2,398 S.F.
(N) CONCRETE SIDEWALK	2,130 S.F.
(N) TRASH ENCLOSURE PAD	288 S.F.

SITE FURNISHING NOTES

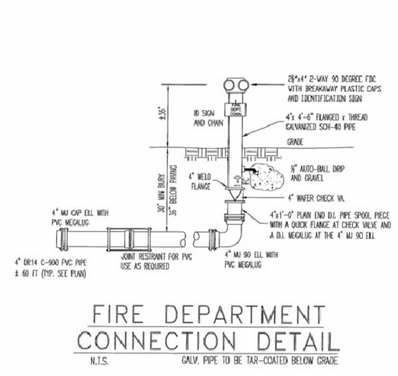
1. 4' BENCH—COMMERCIAL SITE FURNISHINGS 4—FOOT U—LEG PERFORATED METAL COMMERCIAL PARK BENCH w/ SURFACE MOUNT, SKU# CF—FRDVX, COLOR BROWN. (6) TOTAL. SECURE w/ 4—3"x3/8" ANCHOR BOLTS SET IN CONCRETE SLAB w/ EPOXY PER MFG. SPECS AT EACH LEG. FOR DECOMPOSED GRANITE PATIO APPLICATIONS POUR 8" DIAMETER SONOTUBE FOOTING 8" DEEP @ EACH LEG & SET FLUSH w/ ADJACENT SURFACE. SECURE BENCH LEGS AS ABOVE.
2. PICNIC TABLE—COMMERCIAL SITE FURNISHINGS 6—FOOT STANDARD PERFORATED METAL PICNIC TABLE w/ SURFACE MOUNT, SKU# CF—XWZH, COLOR BROWN. (8) TOTAL
3. ACCESSIBLE PICNIC TABLE—COMMERCIAL SITE FURNISHINGS 8—FOOT STANDARD EXPANDED METAL PICNIC TABLE WITH HANDICAP ACCESS & SURFACE MOUNT, SKU# CF—QC6NB, COLOR BROWN. (2) TOTAL

CONTACT INFORMATION FOR SITE FURNISHINGS:

COMMERCIAL SITE FURNISHINGS 1—800—510—8325, commercialsitefurnishings.com

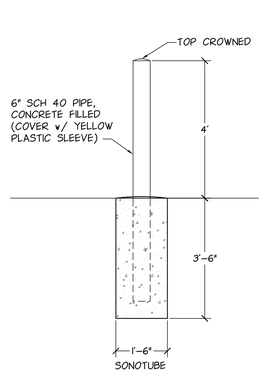
NORTH

16 8 0 16
SCALE: 1"=16'
CAUTION: GRAPHIC SCALE MUST BE USED EXCEPT WHERE DIMENSIONED



FIRE DEPARTMENT CONNECTION DETAIL
N.T.S. GALV. PIPE TO BE TAR-COATED BELOW GRADE

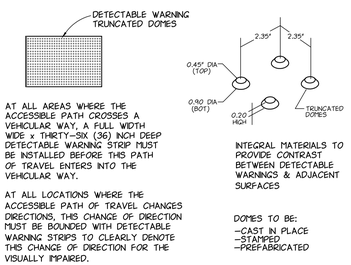
DETAIL 'D'
NOT TO SCALE



DETAIL 'E'
BOLLARD DETAIL
NOT TO SCALE

PATH OF TRAVEL NOTES:

—SLOPE NTE 4.9% IN LINE OF TRAVEL
—CROSSPITCH NTE 1.9%
—SLOPE NTE 1.9% IN ANY DIRECTION FOR LANDINGS & PARKING AREAS AS NOTED



AT ALL AREAS WHERE THE ACCESSIBLE PATH CROSSES A VEHICULAR WAY, A FULL WIDTH WIDE x THIRTY—SIX (36) INCH DEEP DETECTABLE WARNING STRIP MUST BE INSTALLED BEFORE THIS PATH OF TRAVEL ENTERS INTO THE VEHICULAR WAY.

AT ALL LOCATIONS WHERE THE ACCESSIBLE PATH OF TRAVEL CHANGES DIRECTIONS, THIS CHANGE OF DIRECTION MUST BE BOUNDED WITH DETECTABLE WARNING STRIPS TO CLEARLY DENOTE THIS CHANGE OF DIRECTION FOR THE VISUALLY IMPAIRED.

INTEGRAL MATERIALS TO PROVIDE CONTRAST BETWEEN DETECTABLE WARNINGS & ADJACENT SURFACES

DOMES TO BE:
—CAST IN PLACE
—STAMPED
—PREFABRICATED

DETAIL 'F'
TRUNCATED DOME DETAIL
NOT TO SCALE

KEY

- CONCRETE PAVING (NEW), SEE— C L-7
- NEW PAVERS IN MORTAR BED, SEE A-A L-11
- NEW PERMEABLE PAVERS, SEE— A-A L-11
- COMPACTED DECOMPOSED GRANITE, SEE NOTES L—6
- RESURFACE PAVING: PATCH REPAIR POTHOLES, SLURRY SEAL AS SHOWN IN PARKING & DRIVE AREAS. SEE SHEET L—13 FOR SPECIFICATIONS
- EXISTING ASPHALT PAVING TO REMAIN
- EXISTING ASPHALT PAVING TO BE REMOVED
- NEW FLUSH CURB, SEE— J L-7
- NEW RAISED CURB, SEE— H L-7
- POLE LIGHT (7), SEE— Q L-8
- BENCH (6)—SEE NOTE 1 THIS SHEET
- PICNIC TABLE (8)—SEE NOTE 2 THIS SHEET
- ADA PICNIC TABLE (2)—SEE NOTE 3 THIS SHEET
- BUILT—IN CHARCOAL GRILL, SEE— T L-9
- RAISED GARDEN PLANTERS (15) SEE— A L-7
- 6' ORNAMENTAL IRON FENCE R L-7
- 16" SQ. SLUMP—STONE PILASTERS ALONG NEW FENCELINE @ 24' O.C. SEE— E L-7
- ORNAMENTAL IRON FENCE w/2"x2" METAL POSTS @ 8' O.C. R L-8
- SURE—LOCK STEEL EDGING, HEADER, SEE— B L-7
- ADA PATH OF TRAVEL—SEE NOTES THIS SHEET
- EXISTING GAS METER
- ADA PARKING SIGN PER Z L-9
- TRUNCATED DOMES PER F L-2

NOTE
BAR SHOULD MEASURE 1" OR DRAWING IS NOT TO SCALE
0 1/2 1

NO.	DATE	ISSUED FOR	BY

LAST EDIT DATE:

SCALE:
DESIGNED:
DRAWN: JRS
CHECKED:
APPROVED:

CAD REF. FILE:

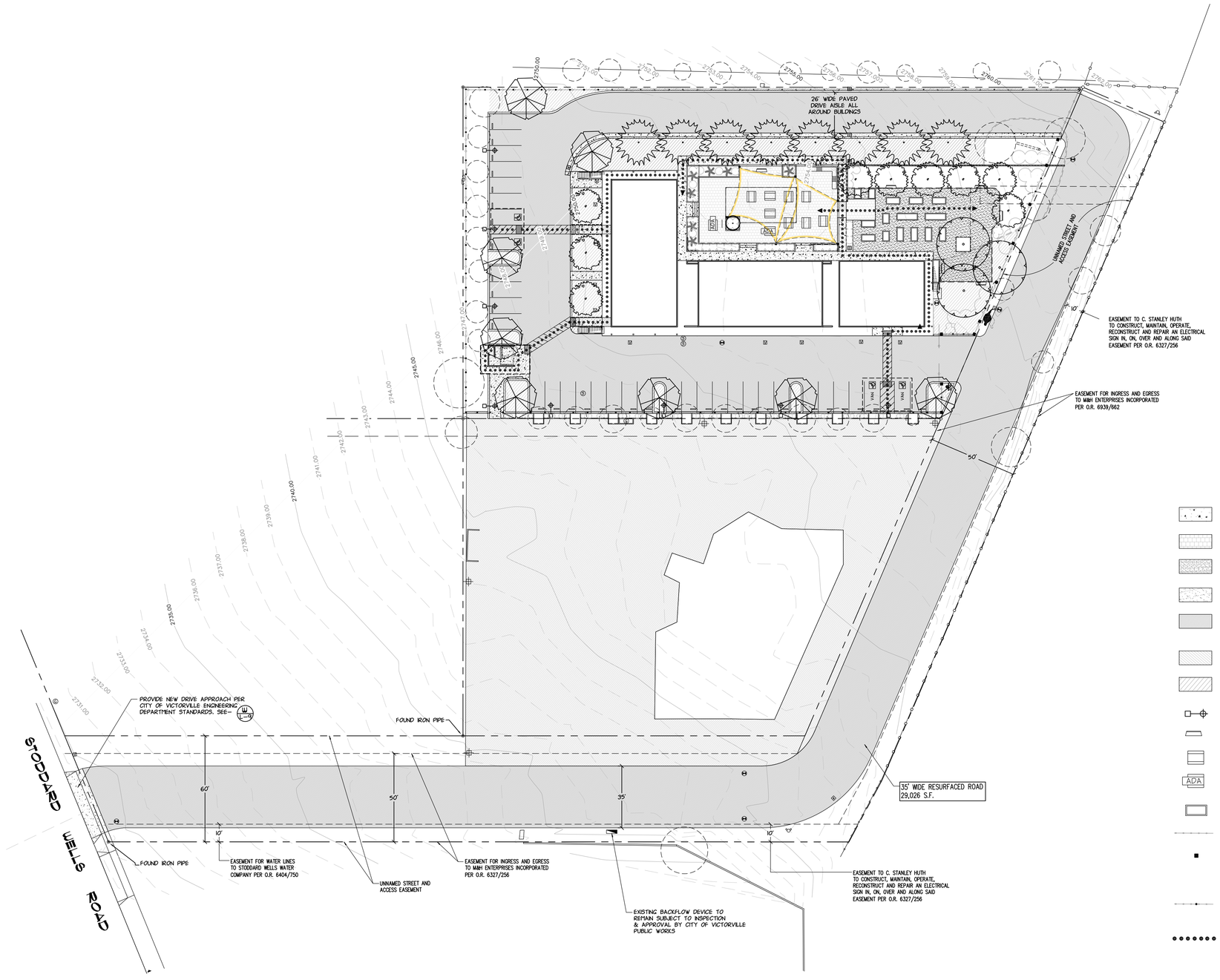
Bisnett Design Associates
bda
16646 Bear Court
Victorville, CA 92394
(530)277-3725
brian@bisnettdesign.com
Landscape Architecture & Environmental Planning

DESERT HAVEN APARTMENTS
VICTORVILLE, CALIFORNIA

SITE PLAN

6-11-18

DRAWING::
L-2



KEY

- CONCRETE PAVING (NEW), SEE DETAIL
- NEW PAVERS
- NEW PERMEABLE PAVERS
- COMPACTED DECOMPOSED GRANITE
- RESURFACE PAVING: PATCH REPAIR POTHOLES, SLURRY SEAL AS SHOWN IN PARKING & DRIVE AREAS. SEE SHEET L-13 FOR SPECIFICATIONS
- EXISTING ASPHALT PAVING TO REMAIN
- EXISTING ASPHALT PAVING TO BE REMOVED
- POLE LIGHT (7)
- BENCH (6)
- PICNIC TABLE (8)
- ADA PICNIC TABLE (2)
- RAISED GARDEN PLANTERS (15)
- 6' ORNAMENTAL IRON FENCE
- 16" SQ. SLUMP-STONE PILASTERS ALONG NEW FENCELINE @ 24" O.C.
- ORNAMENTAL IRON FENCE w/2"x2" METAL POSTS @ 8' O.C.
- ADA PATH OF TRAVEL

NOTE
0 1/2 1
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1	4-17-18	FOR REVIEW	

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Bisnett Design Associates
bda
16046 Bear Court
Victorville, CA 92394
530.277.9755
brian@bisnettdesign.com
Landscape Architecture & Environmental Planning

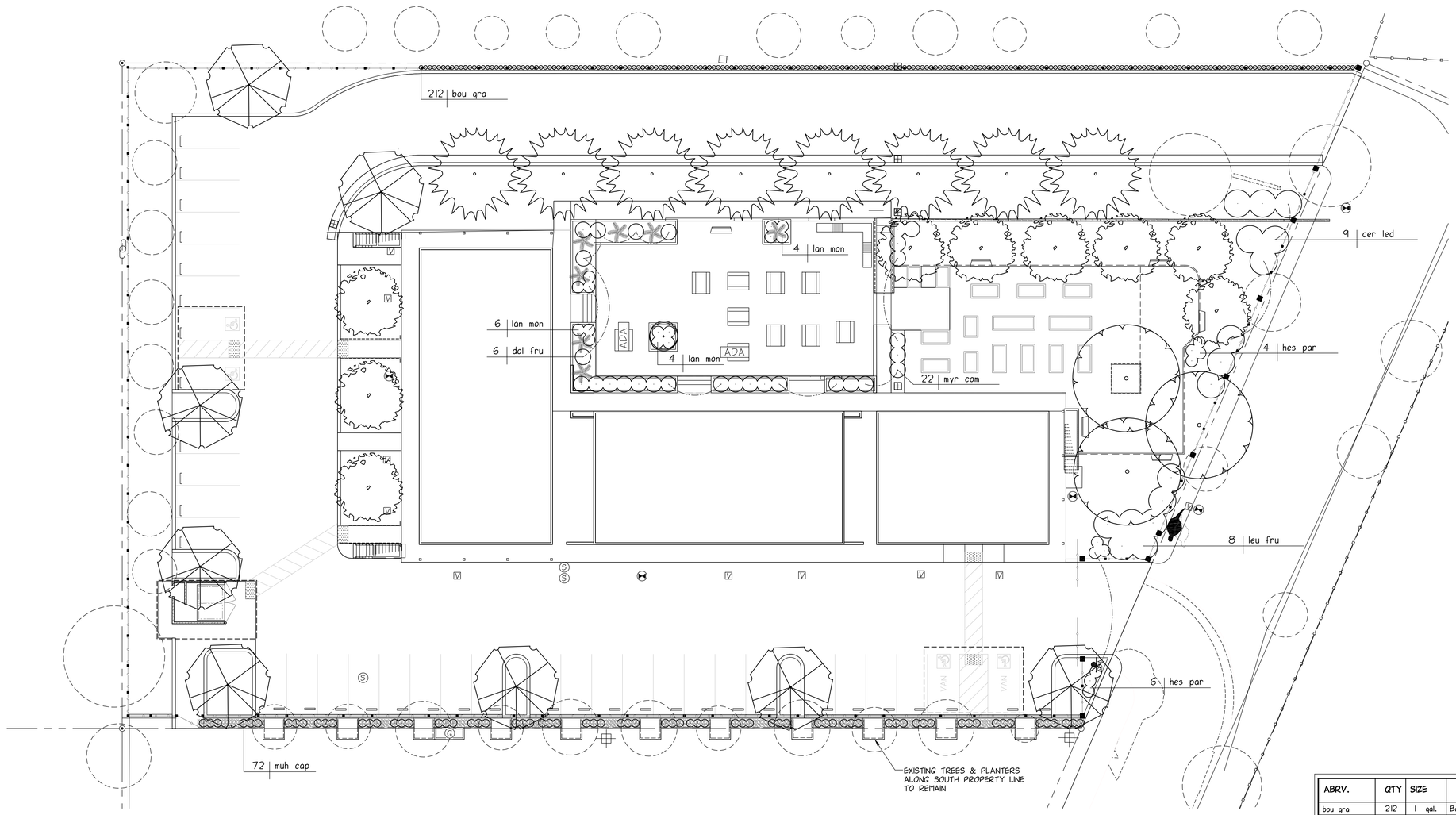
DESERT HAVEN APARTMENTS
VICTORVILLE, CALIFORNIA

SITE PLAN
WITH OFF-SITE IMPROVEMENTS

6-11-18

DRAWING::

L-3



NORTH

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SCALE: 1"=16'
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PLANT LEGEND

ABRV.	QTY	SIZE	PLANT NAME	COMMON NAME	OC'	WATER USE
bou gra	212	1 gal.	Bouteloua gracilis	Blue Grama Grass	15"	L
cer led	9	5 gal.	Cercocarpus ledifolius	Curl-LeafMountain Mahogany	9'	L
dal fru	6	1 gal.	Dalea frutescens	Black Dalea	4'	L/M
hes par	10	1 gal.	Hesperaloe parviflora	Red Yucca	3'	L/M
lan mon	14	1 gal.	Lantana montevidensis	Lantana	3'	M
leu fru	8	5 gal.	Leucophyllum frutescens	Texas Ranger	7'	L/M
muh cap	72	1 gal.	Muhlenbergia capillaus	White Awn Muhly	2'	L
myr com	22	5 gal.	Myrtus communis	Myrtle	4'	L/M

ANY DISCREPANCY BETWEEN THE MAXIMUM SPACING CRITERIA AND THE NUMBER OF PLANTS SHOWN ON THE LANDSCAPE PLAN SHALL RESULT IN THE SPACING CRITERIA SUPERSEDING ANY OTHER INFORMATION SHOWN ON THE LANDSCAPE PLAN.

TREE LEGEND

SYMBOL	QTY	SIZE	PLANT NAME	COMMON NAME	WATER USE
	9	15 gal.	Cercidium 'Desert Museum'	Thornless Palo Verde	L/M
	7	24" box	Chamaerops humilis	Mediterranean Fan Palm	M
	8	15 gal.	Chitalpa tashkentensis 'Morning Cloud'	Chitalpa	L/M
	3	15 gal.	Quercus suber	Cork Oak	L/M
	8	15 gal.	Pinus eldarica	Afghan Pine	L/M
	1	24" box	Punica granatum 'Tanyosho'	Flowering Pomegranate	M

NO.	DATE	BY
1	4-17-18	FOR REVIEW

ISSUED FOR

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Morgan Hill, CA 95949
530.277.3753
brian@bisnettdesign.com
Landscape Architecture & Environmental Planning

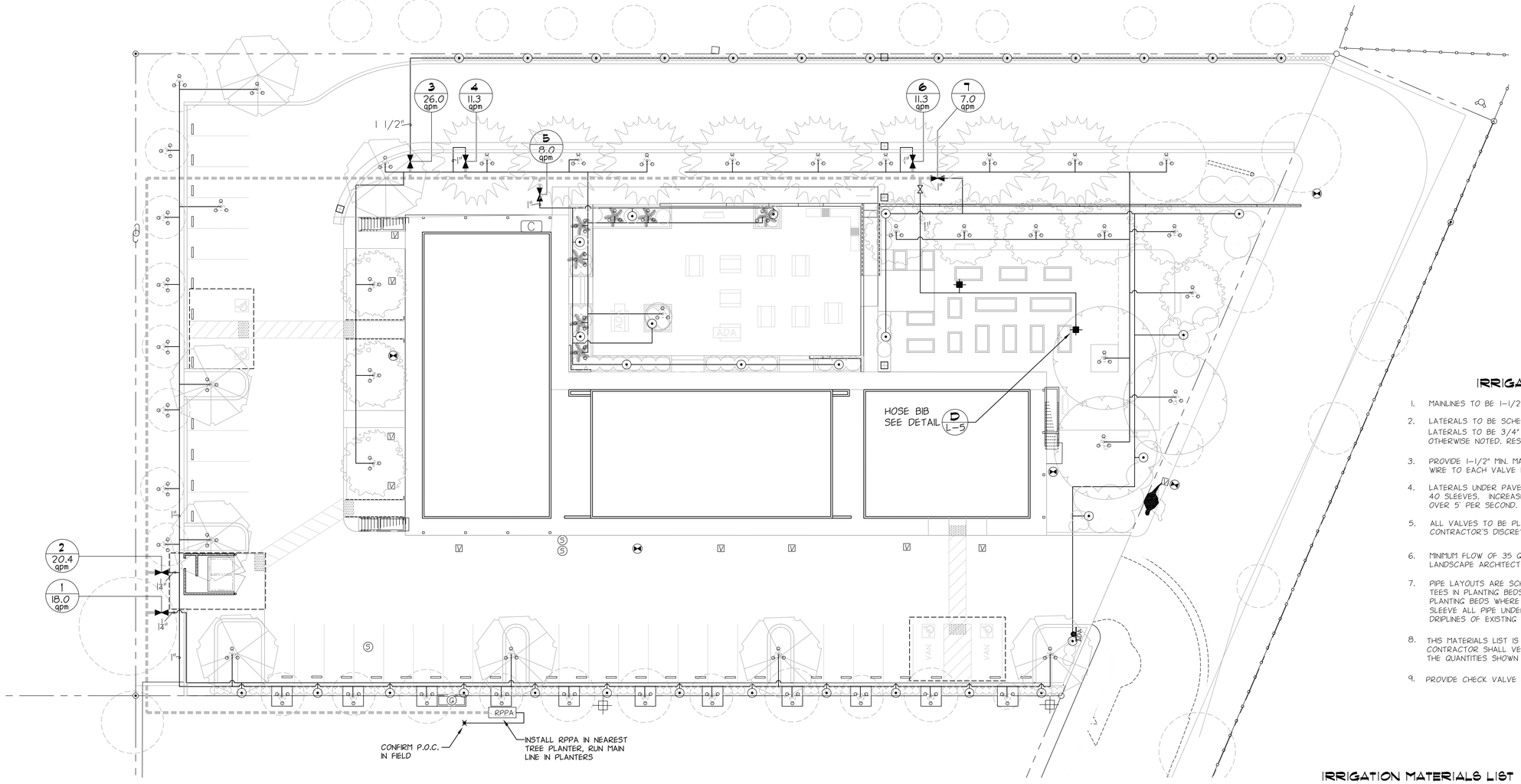
DESERT HAVEN APARTMENTS
VICTORVILLE, CALIFORNIA

PLANTING PLAN

6-11-18

DRAWING::

L-4



IRRIGATION NOTES

1. MAINLINES TO BE 1-1/2" SCH 40 PVC.
2. LATERALS TO BE SCHEDULE 40 PVC.
   LATERALS TO BE 3/4" MIN TO INDIVIDUAL HEADS UNLESS OTHERWISE NOTED. REST OF LINE MIN. 1" UNLESS OTHERWISE NOTED.
3. PROVIDE 1-1/2" MIN. MAINLINE STUB & MIN. 1 ADD'L CONTROLLER WIRE TO EACH VALVE LOCATION.
4. LATERALS UNDER PAVEMENT TO BE SCH. 40 IN OVERSIZED SCH. 40 SLEEVES. INCREASE PIPE SIZING AS REQUIRED TO AVOID FLOWS OVER 5' PER SECOND.
5. ALL VALVES TO BE PLACED IN ONE OR MORE VALVE BOXES AT CONTRACTOR'S DISCRETION.
6. MINIMUM FLOW OF 35 GPM @ 35 PSI ASSUMED. CONTACT LANDSCAPE ARCHITECT IMMEDIATELY IF THIS RATE NOT AVAILABLE.
7. PIPE LAYOUTS ARE SCHEMATIC. LOCATE ALL VALVES, PIPE ELLS AND TEES IN PLANTING BEDS. LOCATE PIPE RUNS ALONG CURBING WITHIN PLANTING BEDS WHERE POSSIBLE. SEE DETAIL 'F', SHEET L-4 SLEEVE ALL PIPE UNDER PAVEMENT. AVOID TRENCHING WITHIN DRIPLINES OF EXISTING TREES.
8. THIS MATERIALS LIST IS PROVIDED FOR CONVENIENCE OF CONTRACTOR. CONTRACTOR SHALL VERIFY ALL COUNTS, AND IF A DISCREPANCY EXISTS, THE QUANTITIES SHOWN GRAPHICALLY ON THE PLAN SHALL GOVERN.
9. PROVIDE CHECK VALVE AT LOWEST POINT OF EACH IRRIGATION CIRCUIT.

IRRIGATION MATERIALS LIST

SYM.	MANUF.	QTY.	MODEL		COMMENTS
RPPA	WATTS/FEBCO	1	009	–	1-1/2" RED. PRES. PRINCIPAL ASSEMBLY PER L-6-'A'
C	BASELINE	1	1000		2-WIRE INTELLIGENT IRRIGATION CONTROLLER w/ STAINLESS STEEL ENCLOSURE, SEE L-6-'B'
	HUNTER	7			101G (1"), OR 151 (1-1/2") SOLENOID VALVE
		40			1" RISER IN 6" VALVE BOX PER L-6-'G'. SHEET L-6
		58			TREE BUBBLERS, PER L-6-'H'
		2			HOSE BIB, PER L-7-'D'
		1			1" GATE VALVE IN PLASTIC VALVE BOX

EXISTING SCH. 40 1-1/2" SCH. 40 PVC MAINLINE REPLACE w/ NEW AS REQUIRED

AS REQ'D: IRRIGATION LATERALS SIZE & WALL THICKNESS PER DWG. & NOTES

NORTH

16 8 0 16
SCALE: 1"=16'
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NO.	DATE	BY
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Landscape Architecture & Environmental Planning

DESERT HAVEN APARTMENTS
VICTORVILLE, CALIFORNIA

IRRIGATION PLAN

6-11-18

DRAWING::
L-5

PLANTING
GENERAL
REVIEWS: The Contractor shall specifically request two working days in advance the following reviews prior to progressing with the work: plant material approval and layout, substantial completion, and final review.
NOTIFICATIONS OF PLANT MATERIAL: Within 7 days after award of contract, Contractor shall submit to Landscape Architect certification of the quantity and species of plant material ordered, the nursery supplying the material and list of plant material not available. Landscape Architect must approve proposed substitutions.
QUALITIES: Contractor shall furnish and install all plants shown schematically on the drawings. Plant list is for use of clients and Landscape Architect and does not represent complete lists of the contract.
ROOT SYSTEMS: All container-grown plant material shall be grown in the container for at least six months prior to the planting, and shall be well established in the container. Contractor shall allow one percent of the plant quantity for removal and inspection. Any plant material which is determined by the Landscape Architect within one year following final acceptance of the project to be defective, declining, root bound or otherwise deficient due to abnormal root growth, shall be replaced by the Contractor with specimens equivalent in development to adjacent plantings.
TREES: All trees shall have straight, uniformly tapered trunks free of damaged bark, with all minor abrasions and cuts showing healing tissue. Trees unable to stand upright without support shall be rejected. Sucker basal and lateral growth shall be removed.
HEALTH: Foliage, roots and stems of all plants shall be vigorous, healthy and normal for species. All plants to be free of disease, harmful insects, burns or other disfiguring characteristics.
UNTRUE SPECIES: All plant material determined by the Landscape Architect to be untrue to the species, clone and/or variety specified within the first year following acceptance of the project, shall be replaced by the Contractor with specimens equivalent in development to adjacent plantings.
SOIL PREPARATION
DESCRIPTION: Work in this section includes the installation of topsoil and soil amendments, finish grading, preparation of planting pits and related incidental work.
REVIEWS: The Contractor shall specifically request two working days in advance the following reviews prior to progressing with the work: rough grading, planting pits for box trees, finish grade.
NOTIFICATION OF AMENDMENTS: Contractor shall provide samples of written certificates stating quantity, volume, composition and origin for all amendments, chemicals and import soils before said material is used on the site.
PRODUCT SPECIFICATIONS:
NATIVE SOIL: Shall be the soil existing on the site prior to work after all rocks over one-and one-half inches in diameter and all foreign matter have been removed.
IMPORTED TOPSOIL: Shall be free of chemicals harmful to plant growth, weed seed and growth, harmful insects, rocks over one-and-one-half inches in diameter and clods over 4 inches in diameter, or which will not break up during installation. Soil shall conform to USDA classification for sandy loam.
SOIL AMENDMENT: Shall be a nitrified, composted sawdust or rice-hull-based product approved by Landscape Architect prior to delivery on site.
FERTILIZER: For container-grown plant stock use controlled-release 'Bio-Pak' fertilizer packets, 10-gram 16-16-8 NPK with sulphur, iron, magnesium and zinc. For ground cover and lawn areas use urea-based resin coated fertilizer, 16-7-12 NPK with iron.
PLANTER SOIL MIX: To be a bulk UC-type planting mix or equivalent as approved by Landscape Architect.
MULCH:
To be chipped fir bark screened 1/4"-1", 3" deep as approved by Landscape Architect.
PRE-EMERGENTS: Apply Treflan, Ronstar G or approved equivalent to area of work after planting as per manufacturers instructions. Apply second application at the end of the maintenance period.
ROOT FUNGICIDES: Treat rootballs of all California natives with Benlate as per manufacturers instructions or approved alternative during transplanting.
ANTI-DESICCANTS: Apply Wilt-pruf NCF or approved equivalent as per manufacturers instructions to all trees immediately after planting.
WARRANTY: The contractor shall be responsible for all plant material and workmanship for a period of 90 days, except as noted, following date of substantial completion. At the end of the period any plant that is dead or has not shown satisfactory growth due to Contractor's installation or negligence shall be replaced at Contractor's expense with material originally specified. Contractor shall not be held liable for plants damaged or destroyed by vandalism or Acts of God.
TIME LIMITS: The maintenance period shall commence from the date of substantial completion and extend for 90 days thereafter.
REQUIREMENTS: All planting areas shall be kept weed-free at all times during the maintenance period. Disease, insect and pest control shall be the Contractor's responsibility. All planting areas shall be kept at optimum moisture for plant growth. Planting not adequately irrigated by automatic irrigation system shall be hand watered as needed and irrigation system adjusted as necessary to maximize performance. Erosion, or settlement of soil, sod or plantings shall be repaired by contractor, and dead or dying plants replaced promptly as their condition becomes apparent.
OWNER'S RESPONSIBILITY: Work installed under this contract damaged by vandalism, vehicular traffic, and/or theft prior to substantial completion shall be replaced by Contractor without cost to Owner. Subsequent to substantial completion, losses due to vandalism, vehicular traffic, and/or theft shall be the responsibility of the Owner to repair or replace.
TURF GRASS: Contractor shall provide all services and incidental work necessary to promote and maintain a hardy, well-established turf during the maintenance period. Bare or weak spots should be promptly resodded. Turf grass should be mowed to a two inch height after it first exceeds a three inch height and mowed at this height weekly. Grass clippings shall be removed from the site.
SUBSTANTIAL COMPLETION: Occurs at time when Landscape Architect certifies all major planting, including seed and ground cover, has been satisfactorily installed, the irrigation is fully operational, mulches and top dressings are in place and all other work is satisfactorily completed, with the exception of minor items compiled by the Landscape Architect for prompt completion by Contractor.
EXECUTION
SOILS TEST: The soil preparation requirement shown below is preliminary for bidding purposes only. A soils test shall be provided by the contractor at time of construction. Soil preparation requirements shall be modified to comply with actual soil preparation recommendations in the soils report. A copy of the soils report shall be submitted prior to approval of the soil amendments per these specifications.
LIMITS AND GRADES: The Contractor shall specifically request two working days in advance prior to progressing with the work a review of existing grades and of the site and work completed to date, and of the soil preparation work to commence. Contractor shall complete the rough grading as necessary to round top and toe of all slopes and to provide natural contours between newly graded areas and the surrounding topography.
TOPSOIL PLACEMENT: Subgrade shall be ripped or tilled until it is loose and friable to depth of six inches. Topsoil shall be spread to six inches in depth and tilled uniformly into subsoil. Remainder of topsoil to be spread, thoroughly water-settled, and brought to smooth even grade in accordance with paragraph entitled 'FINISH GRADING'.
AMENDMENT PLACEMENT: Areas to be planted with perennials or plant materials from flats or six-packs shall have a minimum of four (4) cubic yards of approved amendments per 1000 square feet tilled thoroughly into the top six inches of soil until a homogenous mixture of soil and amendment has been obtained. Lawn areas shall have a minimum of three inches of approved amendment similarly installed. Fertilizer shall be applied over amended areas at the rate of twenty pounds per one thousand square feet prior to tilling.
BACKFILL of PLANT PITS: Backfill for plant pits shall be one part approved amendment to two parts existing soil for rhododendron, azaleas and other acid-loving plantings of the ericaeae family, and one part approved amendment to three parts existing soil for other plantings. Plant pits shall have their sides and bottoms scarified to prevent compaction or glazing. Plant pits shall be dug to twice the width and 1-1/2 times the depth of the rootball to be planted for one, five and fifteen gallon plantings. Boxed specimens shall have a minimum of twelve inches of backfill all around. Pits shall be filled with amended backfill to height required for planting, filled with water, and permitted to drain. Verify plantings are at or slightly above finish grade.

FERTILIZER: Container plants shall receive fertilization at the rate of one packet per container gallon.
1. Specimen plants shall receive fertilization at the above rate or at 5 packets per caliper inch, whichever is less.
2. Space packets evenly around the rootball halfway up (or at varying depths for fifteen gallon and boxed specimens) without touching sides of rootball.
3. Acid loving plants of family ericaceae such as Rhododendron, Azalea, Pieris, Camellia and Erica shall receive acid fertilizer as per manufacturers specifications.
TRANSPLANT PROCEDURES:
1) Soak area within drip line of plant thoroughly, 48 hours before transplanting small specimen and 96 hours before transplanting large specimen trees. Verify that plant is not drought stressed and moisture remains throughout root zone.
2) Prepare planting hole as per regular planting specifications.
3) Prune back 30%-50% of the foliage, depending on type of planting and season.
4) Spray remaining foliage with 'Wiltpruf' or approved equal anti-desiccant.
5) Trench all around rootball and secure if necessary. Underdig rootball and lift, with sling if necessary, and place in new plant pit. Do not allow rootball to break.
6) Place rootball at or above grade. Trees should be placed with crowns minimum 2" above finish grade.
7) Fertilize as per above specs.
8) Backfill around rootball and build watering basin.
9) Soak thoroughly.
10) Stake trees to prevent movement of rootball as per tree detail.
FINISH GRADING: Contractor shall finish all planting areas, and shall remove all rocks and clods over 1" in diameter from surface. All erosion damage occurring during construction period shall be repaired, and all areas should be smooth and evenly graded. Unless otherwise noted, all soil finish grades shall be one inch below finish elevations of walks, pavement and curbs. Grade all finish surfaces smooth and even with positive drainage to swales or drain inlets.

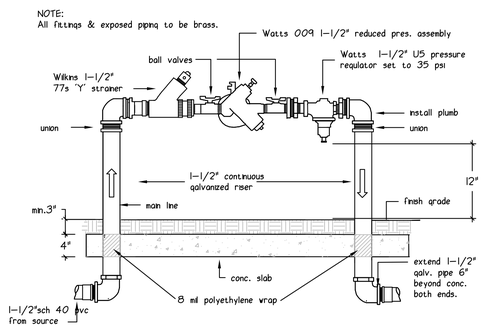
SODDING PROCEDURES:
1) Soil Preparation: As per above.
2) Grading and Rolling: Carefully smooth all surfaces to be sodded. Roll area to expose soil depressions or surface irregularities. Regrade as required.
3) Fertilizing: Spread turf starter fertilizer onto the soil evenly at the rate of four (4) pounds per 1,000 square feet of lawn area.
4) Laying sod: Lay first strip of sod along a straight line (use a string in irregular areas.) Butt joints tightly, but do not overlap edges. On second strip, stagger joints. Use a sharp knife to cut sod to fit curves, edges and irrigation heads.
5) Watering: Do not lay entire lawn before watering. When a conveniently large area has been sodded, water lightly to prevent drying. Continue to lay sod and to water until installation is complete.
6) Rolling sod: After laying all sod, roll lightly to eliminate irregularities and to form good contact between sod and soil. Avoid a very heavy roller or excessive initial watering which may cause roller marks.
7) Irrigation: Water thoroughly the completed lawn surface. Soil should be moistened at least eight (8) inches deep. Repeat irrigation at regular intervals to keep sod moist at all times until rooted. After sod is established, decrease frequency and increase amount of water per application as necessary.
8) Replacement: Replace all dead or dying sod with equal material as directed by the Landscape Architect.
FINAL REVIEW: Contractor shall request a final review of the project within five (5) days in advance of the proposed date, and subsequent to the completion of the maintenance period. Failure to request this review will automatically postpone the date of completion, and lengthen the maintenance period, until final review is approved.
MAINTENANCE: All landscaping material shall be maintained in a healthy and weed free condition; dead plant material shall be replaced immediately. All trees shall be maintained and pruned in accordance with the accepted practices of the International Society of Arboriculture (ISA).

IRRIGATION:
GENERAL
DESCRIPTION: Work in this section includes installation of a complete automatic irrigation system, including trenching, piping, valves, back flow prevention device, controller, pressure regulators, sprinklers, emitters and other components and incidentals related thereto.
QUALITY: All materials shall be new and of the quality specified or better. All materials shall be clearly marked by manufacturer on material, containers, or certificates of contents.
PIPE AND FITTINGS: Mainlines to be Schedule 40 Polyvinyl chloride pipe (PVC). Lateral lines to be Schedule 40 PVC. Weldon PVC primer or equal to be used according to manufacturers directions on Schedule 40 pipe connections. Do not use on class pipe. All fittings shall be Schedule 40 heavy wall or thicker. Use solvent as recommended by pipe manufacturer. Use Teflon tape on all threaded joints. For above ground mainline connections use galvanized pipe, for above ground laterals use schedule 80 pipe (with clear cement) or galvanized pipe. For sprinkler heads use schedule 80 nipples with marlex ells triple swing.
CONTROL WIRE: To be 16 gauge, 24 volt solid UA approved for direct burial. Splices shall be made in valve boxes only and shall be "Scotch-lok" seal packs or equal according to manufacturers instructions.
VALVE BOXES: Shall be precast concrete or plastic of type and size indicated. Must be free of cracks and structural defects. Boxes subject to vehicular traffic shall be concrete and have heavy-duty steel covers.
IRRIGATION EQUIPMENT: Refer to drawings. Substitutions must be approved in advance in writing by Landscape Architect.
EXECUTION:
GRADING: Contractor shall install all irrigation features to their finish grade and at depths indicated. All rough grading to be finished or accommodated before trenching begins.
LAYOUT: Layout work as accurately as possible to the drawings. Note that the drawings are diagrammatic. Swing joints, offsets and all fittings are not shown. Pipelines shown parallel on the drawing may be installed in a common trench. Where pipelines are shown parallel or adjacent to shrub or around cover areas they shall be installed in those areas. Where pipelines are shown alongside the intersection of lawn and pavement areas they shall be installed in the lawn area. Changes in depth of pipe shall be accomplished with 45-degree fittings. Mainlines to be buried to 24" minimum depth, laterals to 12" minimum, except where otherwise noted.
FABRICATION: Snake pipe from side to side where trenches exceed 30 feet in length. Manifolds to be constructed to allow valves and boxes to be arranged neatly, uniformly and parallel with adjacent surfaces where appropriate.
THRUST BLOCKS: Contractor to install thrust blocks on mains at all right angle bends, changes in grade and other points as recommended by manufacturer to protect pipe from damage where project pressure exceeds 70 psi.
CONTROL WIRE: Control wire to be taped to irrigation line every 10 feet where practical or placed in class 200 or thicker conduit. Place in steel conduit where installed above finish grade.
ADJUSTMENTS: Contractor to adjust arc, radius, height and distribution of all sprinkler heads for maximum performance of system without additional cost to owner.
BACKFILL: Backfill with approved native or imported topsoil. Use no debris in backfill. No rocks greater than 1" diameter may be in direct contact with pipe. Compact backfill to prevent settling.
FINISH GRADE: Set all heads at finish grade on polyethylene cut-off risers or swing joints as noted. Top of all valve boxes to be flush with finish grade.
CONTROLLER: Contractor to clearly label and sequence stations as they are located around the site. Contractor shall submit forms as necessary to execute the controller guarantee, and shall provide owner with same. Contractor to provide owner with information regarding operation and adjustment of controller, valves and sprinkler heads.
AS-BUILT: Contractor shall keep a record of all changes to the system as they are made throughout the project, and shall provide owner with same on completion of project. Features below ground shall be identified with at least two measurements from above ground reference points, such as walls, walks or sprinkler heads.
VERIFICATION: System design is based on 30 gpm and 60 psi being available at the point of connection. If the flow and pressure is not available the Landscape Architect shall be notified in writing prior to commencement of irrigation work. Flows in all pipes not to exceed 5 feet per second. Pipe to be upsized where necessary to avoid flows in excess of 5 feet per second.
UTILITIES: Contractor shall verify location of all on-site utilities and make all notifications as required prior to trenching. Restoration of damaged utilities is the Contractor's responsibility.
CODES: Irrigation system to be installed in accordance with all applicable local codes as well as manufacturers specifications. If there is a discrepancy with the specifications contained herein, Landscape Architect shall be notified promptly by telephone and in writing prior to installation.
CLEAN-UP: Site to be kept neat and debris-free throughout course of project. All surplus material to be removed from site at completion of work and/or when directed by the Landscape Architect.
GUARANTEE: Landscape contractor shall fill and repair all depressions and replace as necessary lawn and planting affected by settling or irrigation trenches for one year following acceptance of the job. Contractor shall also guarantee all materials, equipment and workmanship to be free of defects for a period of one year following acceptance of project, and shall replace and repair any defects at his expense that may be found in that period.

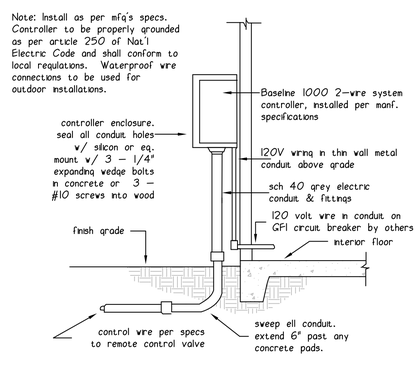
DECOMPOSED GRANITE
Decomposed granite must be crushed granite rock screenings graded from 3/8-inch particles to dust. The material must comply with the following gradation:

Sieve Size	Percent Passing
3/8-inch	100
No. 4	95-100
No. 8	75-80
No. 16	55-65
No. 30	40-50
No. 50	25-35
No. 100	20-25
No. 200	5-15

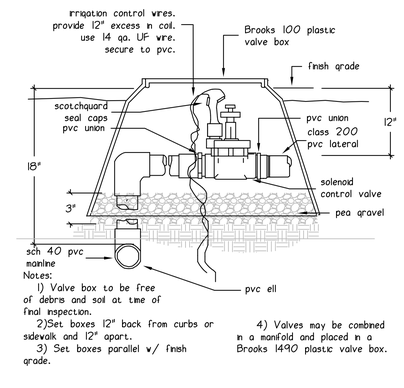
The decomposed granite must be uniform in color and uniform in texture. Color shall be "California Gold" gravel or approved equal.
BINDING MATERIAL/ SOLIDIFYING EMULSION:
Solidifying emulsion must be either a water based polymer or powder based product and be non-toxic, colorless, odorless, and organic in nature. The binder/ solidifying emulsion must not alter the decomposed granite color. "Stabilizer" by Stabilizer Solutions or approved equal, installed per mfg specifications.
EARTHWORK:
After clearing, excavate areas to receive decomposed granite. Where decomposed granite is to be placed adjacent to an existing curb, pavement or sidewalk, excavate so that the finished decomposed granite elevation adjacent to curb, pavement or sidewalk will maintain planned flow lines, slope gradient and contours of the project site. After excavation, grade areas to receive decomposed granite to a smooth, uniform surface and compact to not less than 90 percent relative compaction.
STABILIZED DECOMPOSED GRANITE INSTALLATION
Do not install decomposed granite during rainy conditions.
Mix solidifying emulsion thoroughly and uniformly throughout the decomposed granite per the manufacturer's recommendations. Mix the material in the field using portable mixing equipment, or delivered in mixer trucks from a local ready-mixed plant. Place decomposed granite uniformly no more than 2-inch thick, over 2" 95% compacted class II base rock. Compact the layer of decomposed granite to a relative compaction of not less than 90 percent. Compaction must not begin less than 6 hours after placement, nor more than 48 hours. Apply a final application of binder/ solidifying emulsion as recommended by the manufacturer. Prevent runoff or overspray of binder/ solidifying emulsion onto adjacent paved or planting areas. When work is complete, the surface must be smooth, compacted to 90 percent, and uniform; maintaining original flow lines, slope gradient and contours to the site conditions, with minimum 1.0% slope.



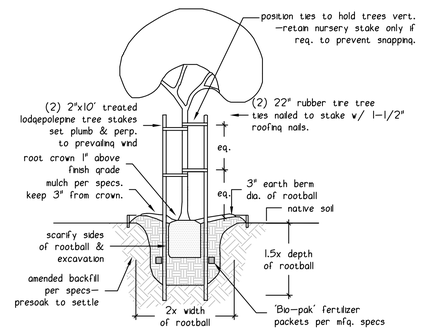
A BACKFLOW DEVICE
NO SCALE



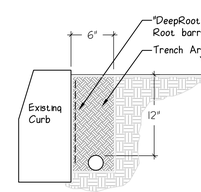
B CONTROLLER
NO SCALE



C VALVE
NO SCALE



D TREE PLANTING DETAIL
NO SCALE



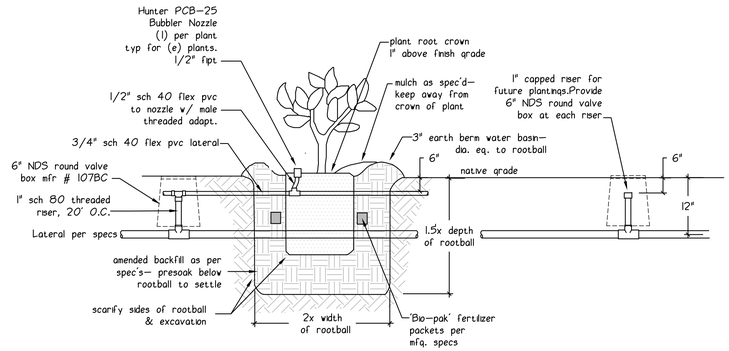
Note -Install laterals within 6" of curb to minimize root damage to existing trees.

-Install "DeepRoot" root barriers along curb line per mfg specs prior to backfill of trench in areas impacted by roots.

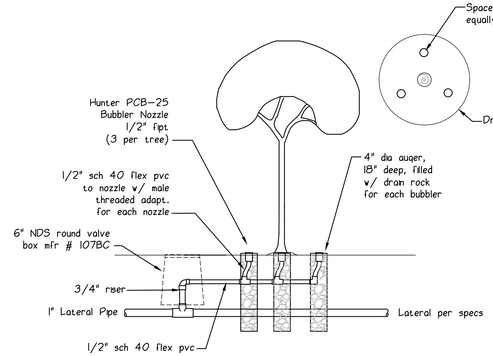
-Flexible PVC may be utilized at contractors discretion to reduce disturbance to existing plant materials. Provide minimum 6" embedment.

-Removal of significant roots (>3" dia) to be performed under direction of licensed arborist

F LATERAL DETAIL
NO SCALE



G SHRUB PLANTING BUBBLERS
NO SCALE



H TREE IRRIGATION
NO SCALE

NOTE
0 1/2 1
BAR SHOULD MEASURE 1" OR DRAWING IS NOT TO SCALE

NO.	DATE	ISSUED FOR	BY
1	4-17-18	FOR REVIEW	

LAST EDIT DATE:
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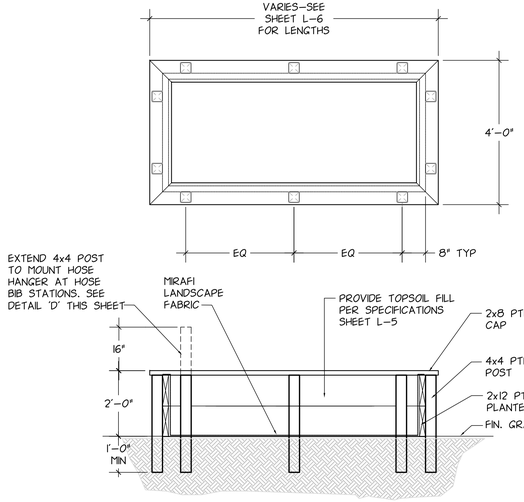
SCALE:
DESIGNED:
DRAWN: JRS
CHECKED:
APPROVED:

Bisnett Design Associates
Landscape Architecture & Environmental Planning
bda
18048 Bear Court
Grass Valley, CA 95949
530.277.9733
brian@bisnettdesign.com

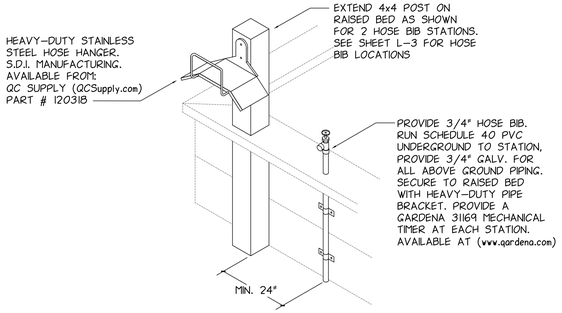
DESERT HAVEN APARTMENTS
VICTORVILLE, CALIFORNIA
IRRIGATION/ PLANTING SPECIFICATIONS & DETAILS

6-11-18

DRAWING::
L-6



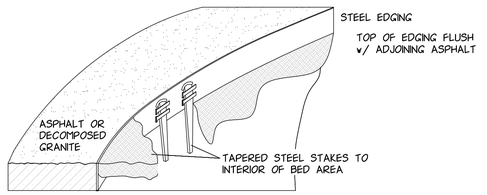
DETAIL 'A'
RAISED GARDEN BED



DETAIL 'D'
HOSE BIB STATION
NO SCALE

sure-loc®

SURE–LOC ALUMINUM EDGING CORPORATION
494 EAST 64TH ST
HOLLAND, MI 49423
TOLL FREE: 1–800–787–3562
PHONE: (616) 392–3209
www.surelocedging.com
FAX: (616) 392–5134



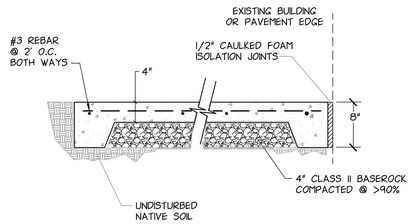
3/16" X 4" X 16' (.182–.188" AVERAGE THICKNESS) FOR ALL CURVED SECTIONS

1/4" X 5" X 16' (.20 – .250" AVERAGE THICKNESS) FOR ALL STRAIGHT SECTIONS

NOTES:

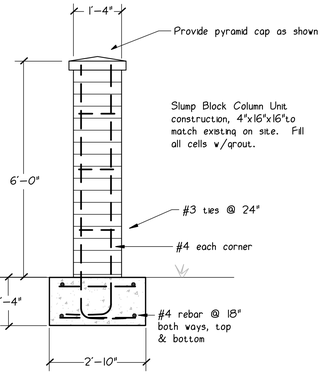
1. INSTALLATION TO BE COMPLETED IN ACCORDANCE WITH MANUFACTURER'S SPECIFICATIONS.
2. CONTRACTORS NOTE: FOR PRODUCT AND PURCHASING INFORMATION VISIT www.CADdetails.com/nfo REFERENCE NUMBER 200–003

DETAIL 'B'
STEEL LANDSCAPE EDGING
NO SCALE

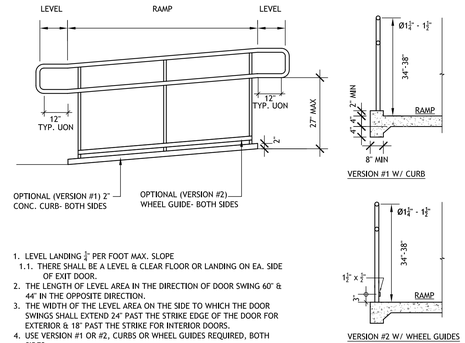


1. CONCRETE CURE TO MIN. 3,000 PSI.
2. FINISH TO MATCH EXISTING PAVING.
3. PROVIDE FATTENED EDGES AT PERIMETER OF SLABS AND SIDEWALKS
4. SLOPE ON SIDEWALKS NOT TO EXCEED 4.9% LONGITUDINAL AND 1.9% CROSS PITCH ALONG ADA PATH OF TRAVEL
5. MIN 1% SLOPE TO BE PROVIDED FOR DRAINAGE ON ALL PAVED SURFACES. SLOPE PAVED SURFACES AWAY FROM EXISTING CONDITIONED SPACE.
6. SLOPE ON PATIO AREAS NOT TO EXCEED 2%.

DETAIL 'C'
CONCRETE DETAIL
NO SCALE

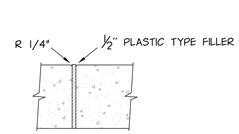


DETAIL 'E'
DECORATIVE COLUMN
NO SCALE



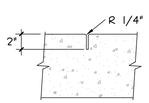
1. LEVEL LANDING ¼" PER FOOT MAX. SLOPE
   1.1. THERE SHALL BE A LEVEL & CLEAR FLOOR OR LANDING ON EA. SIDE OF EXIT DOOR.
2. THE LENGTH OF LEVEL AREA IN THE DIRECTION OF DOOR SWING 60" & 44" IN THE OPPOSITE DIRECTION.
3. THE WIDTH OF THE LEVEL AREA ON THE SIDE TO WHICH THE DOOR SWINGS SHALL EXTEND 24" PAST THE STRIKE EDGE OF THE DOOR FOR EXTERIOR & 18" PAST THE STRIKE FOR INTERIOR DOORS.
4. USE VERSION #1 OR #2, CURBS OR WHEEL GUIDES REQUIRED, BOTH SIDES.

DETAIL 'F'
ADA HANDRAIL DETAILS
NO SCALE

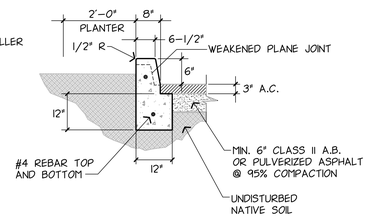


EXPANSION JOINT

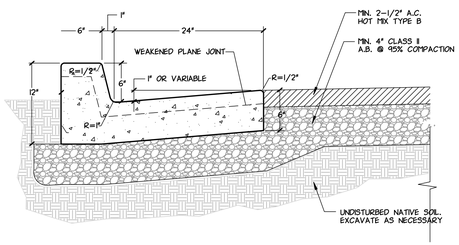
TYPICAL FOR ALL CURBS & CURB & GUTTER



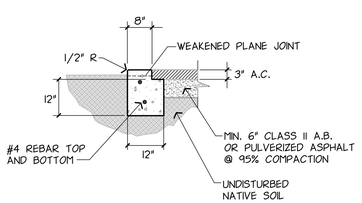
WEAKENED PLANE JOINT



DETAIL 'H'
NEW RAISED CURB
NO SCALE



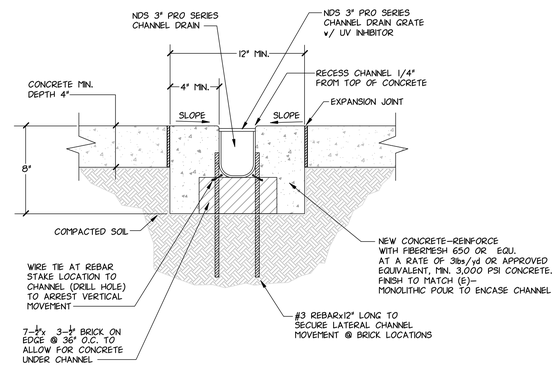
DETAIL 'G'
CONCRETE CURB AND GUTTER
NO SCALE



DETAIL 'J'
NEW FLUSH CURB
NO SCALE

CURB & GUTTER NOTES FOR DETAILS G, H & J:

1. CURBS & CURBS AND GUTTERS SHALL BE CONSTRUCTED FROM PORTLAND CEMENT CONCRETE CONTAINING NOT LESS THAN 550 POUNDS OF TYPE II PORTLAND CEMENT PER CUBIC YARD WITH 4% AIR ENTRAINMENT AND 1" MAXIMUM AGGREGATE GRADING.
2. CONCRETE SHALL BE CURED WITH WHITE PIGMENTED CURING COMPOUND.
3. CURB AND GUTTER SHALL BE CONSTRUCTED ON MINIMUM 4" CLASS 2 AGGREGATE BASE COMPACTED TO 95% RELATIVE COMPACTION.
4. WHEN USED ADJACENT TO VEHICULAR TRAFFIC, CURB SHALL BE CONSTRUCTED ON MINIMUM 4" CLASS 2 AGGREGATE BASE COMPACTED TO 95% RELATIVE COMPACTION.
5. WEAKENED PLANE JOINTS SHALL BE CONSTRUCTED AT 10' INTERVALS.
6. WEAKENED PLANE JOINTS SHALL BE AT LEAST 2" DEEP.
7. EXPANSION JOINTS SHALL BE CONSTRUCTED AT ALL CURB RETURNS, DRIVEWAY APPROACHES AND AT 60' INTERVALS.
8. EXPANSION JOINTS SHALL BE 1/2" WIDE AND FILLED WITH PLASTIC TYPE FILLERS.
9. IF EXISTING CURB IS TO BE REMOVED, IT SHALL BE SCORED AT LEAST 1" DEEP WITH A CONCRETE SAW PRIOR TO REMOVAL. IF THE SAWCUT LINE IS CLOSER THAN 2' TO A WEAKENED PLANE JOINT OR EXPANSION JOINT, THE CURB SHALL BE REMOVED TO THE WEAKENED PLANE JOINT OR EXPANSION JOINT.
10. GUTTER SHALL HAVE ROUGH BROOM FINISH WITH 2" SHINER AT FLOW LINE.



DETAIL 'K'
STRIP DRAIN DETAILS
NO SCALE

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1	4–17–18	FOR REVIEW	

NOTE
0 1/2 1
BAR SHOULD MEASURE 1" OR DRAWING IS NOT TO SCALE

LAST EDIT DATE:

SCALE:
DESIGNED:
DRAWN: JRS
CHECKED:
APPROVED:
CAD/ REF. FILE:

Bisnett Design Associates
bda
16046 Bear Court
Oregon House, CA 95962
530.277.9723
brian@bisnettdesign.com
Landscape Architecture & Environmental Planning

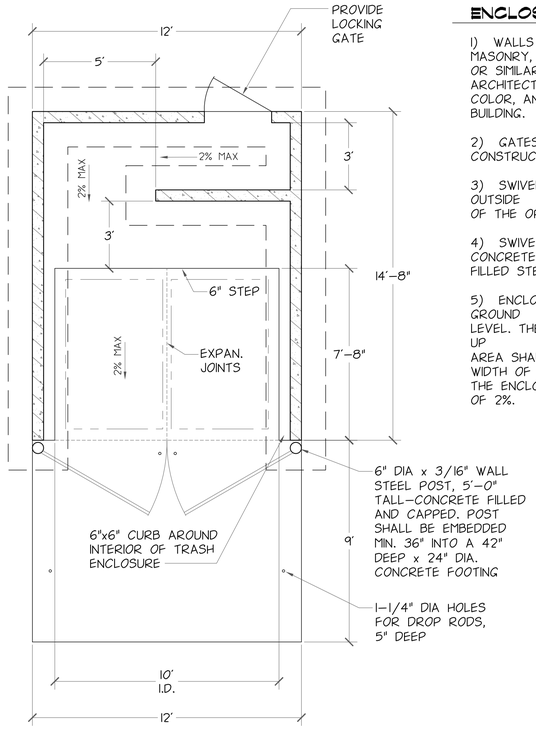
DESERT HAVEN APARTMENTS
VICTORVILLE, CALIFORNIA

CONSTRUCTION DETAILS

6–11–18

DRAWING: :

L–7



ENCLOSURE NOTES:

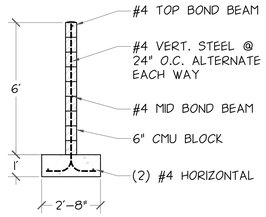
1) WALLS ARE TO BE OF DECORATIVE BLOCK MASONRY, OR SIMILAR MATERIALS CNONSISTANT WITH THE ARCHITECTURE, COLOR, AND ACCENT MATERIALS OF THE PRIMARY BUILDING.

2) GATES ARE TO BE SOLID METAL CONSTRUCTION.

3) SWIVEL POINTS ON DOORS SHALL BE LOCATED OUTSIDE OF THE OPENING AREA OF THE ENCLOSURE.

4) SWIVEL POINTS SHALL BE ATTACHED TO CONCRETE FILLED STEEL POSTS AT ENDS OF WALLS.

5) ENCLOSURE SHALL BE ON A LEVEL PAD AT GROUND LEVEL. THE AREA BETWEEN THE PAD AND THE PICK UP AREA SHALL BE A PAVED AREA 4" DEEP FOR THE WIDTH OF THE ENCLOSURE AND SHALL NOT EXCEED A GRADE OF 2%.

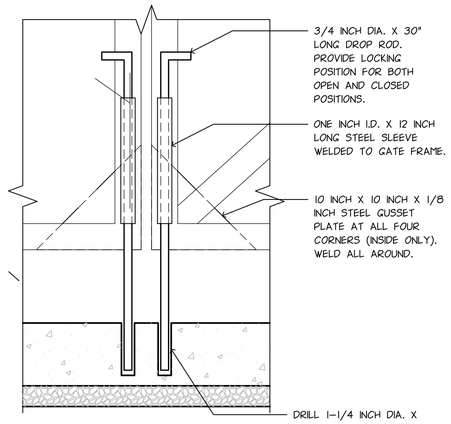


WALL SECTION

ENCLOSURE WALL NOTES:

1) 6" MIN. CMU BLOCK
2) ALL REBAR TO BE #4 MIN.
3) LAP ALL REBAR MIN. 20"
4) MIN. 3" FROM REBAR TO EARTH
5) GROUT ALL CMU CELLS

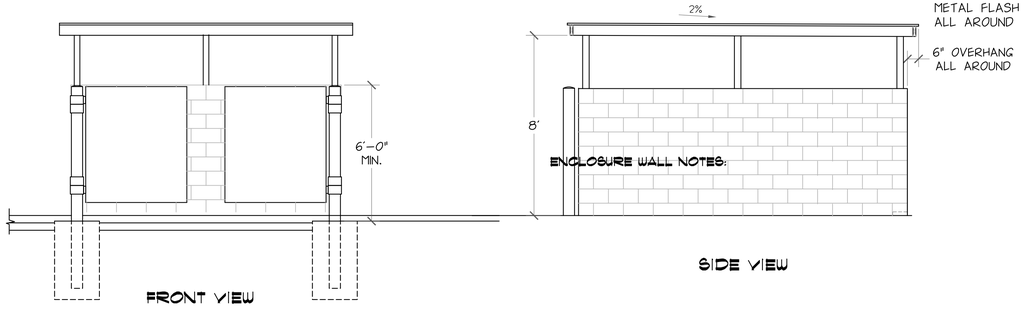
DETAIL 'M'
TRASH ENCLOSURE DETAILS
NO SCALE



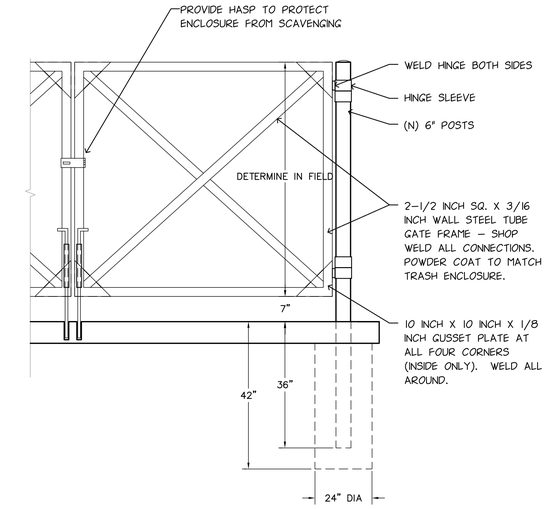
DETAIL 'P'
TRASH ENCLOSURE DETAILS
NO SCALE

SHOP DRAWINGS FOR TRASH ENCLOSURE ROOF TO BE PROVIDED BY GENERAL CONTRACTOR.

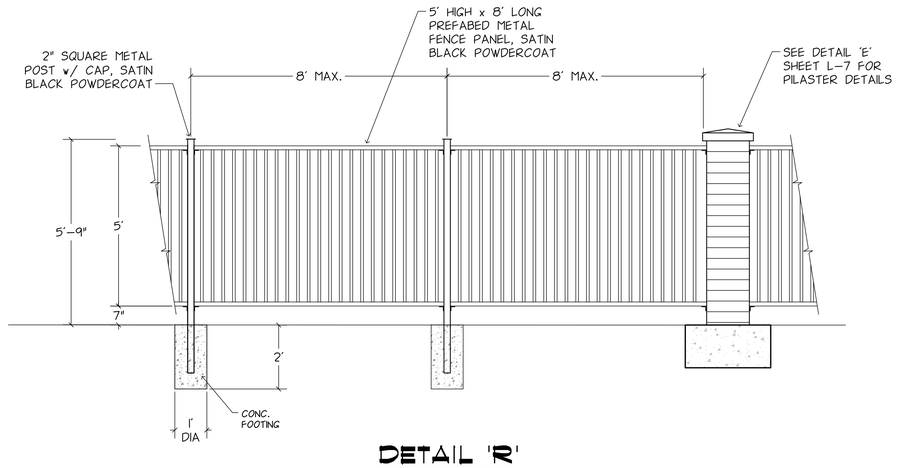
ROOF TO MATCH EXISTING BLDG ROOF



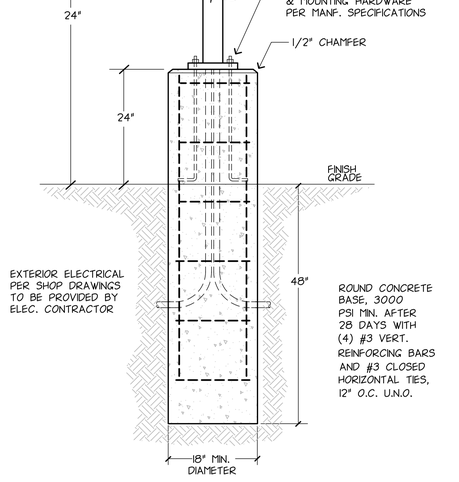
DETAIL 'L'
TRASH ENCLOSURE DETAILS
NO SCALE



DETAIL 'N'
TRASH ENCLOSURE DETAILS
NO SCALE



DETAIL 'R'
ORNAMENTAL METAL FENCING DETAIL
NO SCALE



DETAIL 'Q'
PARKING LOT LIGHTING DETAILS
NO SCALE

NOTE 0 1/2 1
BAR SHOULD MEASURE 1" ON DRAWING IS NOT TO SCALE

NO.	DATE	ISSUED FOR	BY
1	4-17-18	FOR REVIEW	

SCALE:
DESIGNED:
DRAWN: JRS
CHECKED:
APPROVED:
LAST EDIT DATE:
CAD REF. FILE:

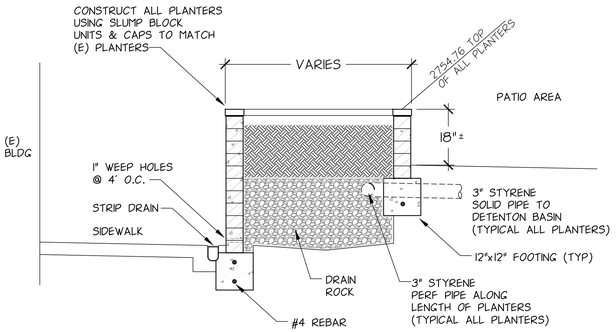
Bisnett Design Associates
bda
19046 Bear Court
Walnut Grove, CA 95690
530.277.5733
brian@bisnettdesign.com
Landscape Architecture & Environmental Planning

DESERT HAVEN APARTMENTS
VICTORVILLE, CALIFORNIA

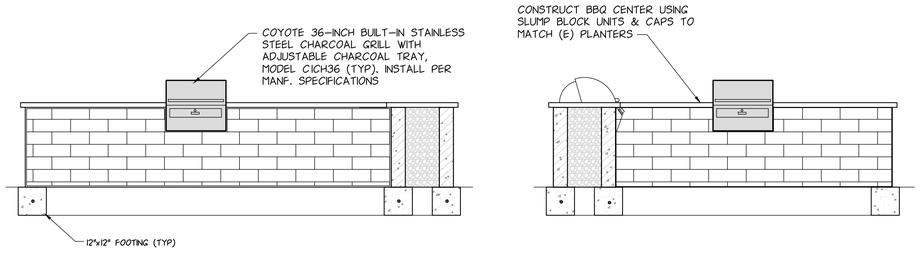
CONSTRUCTION DETAILS

6-11-18

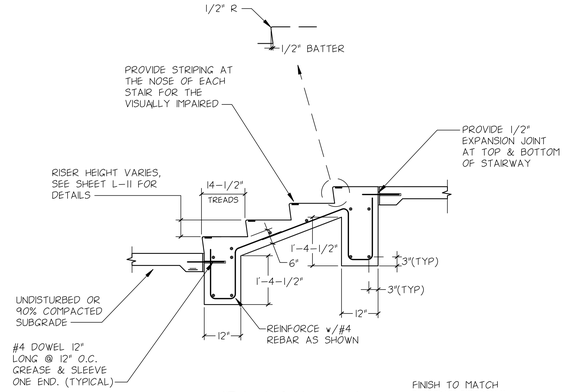
DRAWING:
L-8



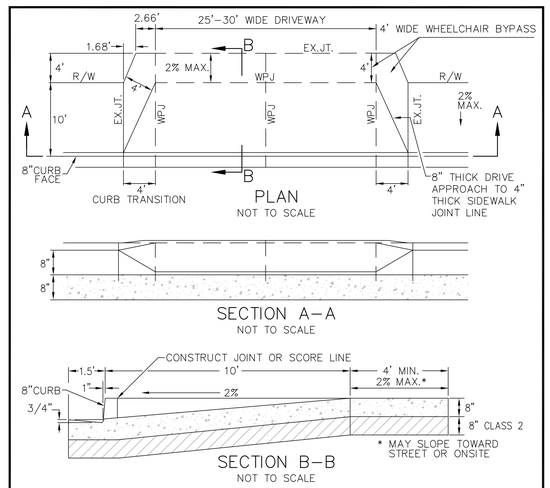
SECTION 'S'
PLANTER DETAILS
NO SCALE



DETAIL 'T'
BBQ CENTER DETAILS
NO SCALE



DETAIL 'V'
NEW CONCRETE STAIR DETAILS
NO SCALE

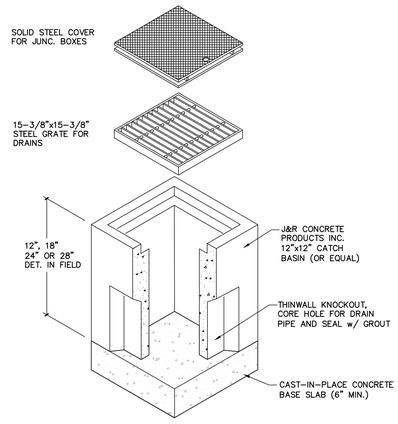


NOTES:

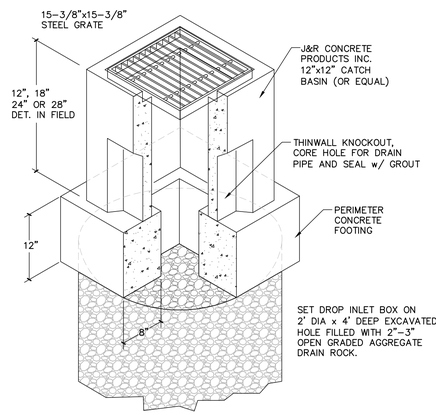
1. COMMERCIAL DRIVE APPROACH SHALL BE CONSTRUCTED FROM PORTLAND CEMENT CONCRETE CONTAINING NOT LESS THAN 550 POUNDS OF TYPE II PORTLAND CEMENT PER CUBIC YARD WITH 4% AIR ENTRAINMENT AND 1" MAXIMUM AGGREGATE GRADING.
2. CONCRETE SHALL BE CURED WITH WHITE PIGMENTED CURING COMPOUND.
3. COMMERCIAL DRIVE APPROACH SHALL BE CONSTRUCTED ON MINIMUM 8" CLASS 2 AGGREGATE BASE COMPACTED TO 95% RELATIVE COMPACTION.
4. WEAKENED PLANE JOINTS SHALL BE AT LEAST 2" DEEP.
5. EXPANSION JOINTS SHALL BE CONSTRUCTED AT BOTH ENDS OF DRIVEWAY APPROACHES.
6. EXPANSION JOINTS SHALL BE 1/2" WIDE AND FILLED WITH PLASTIC TYPE FILLERS.
7. COMMERCIAL DRIVE APPROACH SHALL BE MEDIUM BROOM FINISH.

CITY OF VICTORVILLE - ENGINEERING DEPARTMENT				
REV.	DATE	BY	STANDARD COMMERCIAL DRIVE APPROACH (TYPE 1)	S-03
1	6/1/07	STAFF		
2	11/20/08	STAFF	JOHN A. McGLADE, CITY ENGINEER	SHEET 1 OF 3

DETAIL 'W'
DRIVE APPROACH ON STODDARD WELLS
NO SCALE



DETAIL 'X'
DROP INLET, JUNCTION BOX DETAILS
NO SCALE



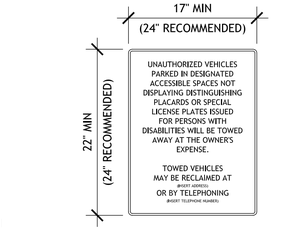
DETAIL 'Y'
DETENTION BASIN OVERFLOW OUTLET
NO SCALE

UNAUTHORIZED VEHICLE WARNING SIGN (CA ONLY)

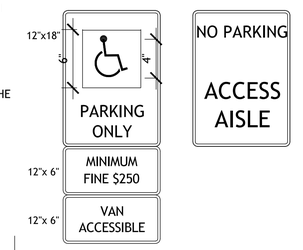
1. SIGN IS TO BE POSTED AT EACH ENTRANCE TO THE OFF-STREET PARKING FACILITIES, OR POSTED AT EACH ACCESSIBLE PARKING STALL.
2. THE PHONE NUMBER AND ADDRESS WHERE TOWED VEHICLES CAN BE RECLAIMED IS POSTED AND A PERMANENT PART OF THE SIGN.
3. LETTERS MUST BE 1" MIN IN HEIGHT.
4. SIGN IS NOT LESS THAN 17" BY 22".

PARKING SPACE SIGNAGE

1. POST SIGNAGE AT EACH ACCESSIBLE SPACE.
2. AREA OF SIGN IS NOT SMALLER THAN 70 SQUARE INCHES.
3. WHEN POSTED IN A PATH OF TRAVEL, BOTTOM OF SIGN IS 80" MIN FROM FINISHED GRADE.
4. POST ADDITIONAL SIGN AT VAN ACCESSIBLE SPACES STATING "VAN ACCESSIBLE" BELOW SYMBOL OF ACCESSIBILITY .
5. REFLECTORIZED SIGN CONSTRUCTED OF PORCELAIN STEEL WITH BEADED TEXT OR EQUAL.
6. SIGN TO BE CENTERED AT THE INTERIOR END OF THE PARKING SPACE.
7. TEXT AND ACCESSIBILITY SYMBOL TO BE WHITE ON DARK BLUE BACKGROUND.
8. POST ADDITIONAL SIGN STATING "MINIMUM FINE $250" BELOW THE SYMBOL OF ACCESSIBILITY FOR ALL CALIFORNIA PROJECTS.



UNAUTHORIZED VEHICLE SIGN
(NOTE: CA ONLY)



MOUNT BOTTOM
60" ABOVE GRADE MINIMUM
EXCEPTION: WITHIN ACCESSIBLE ROUTE
AT 80" ABOVE GRADE

DETAIL 'Z'
ADA SIGNS AT PARKING AREAS
NO SCALE

NO.	DATE	BY
1	4-17-18	FOR REVIEW

ISSUED FOR

NOTE
0 1/2 1
BAR SHOULD MEASURE 1" ON DRAWING IS NOT TO SCALE

LAST EDIT DATE:

SCALE:
DESIGNED:
DRAWN: JRS
CHECKED:
APPROVED:
CAD REF. FILE:

Bisnett Design Associates
bda
16046 Bear Court
Victorville, CA 92840
530.277.9733
brian@bisnettdesign.com
Landscape Architecture & Environmental Planning

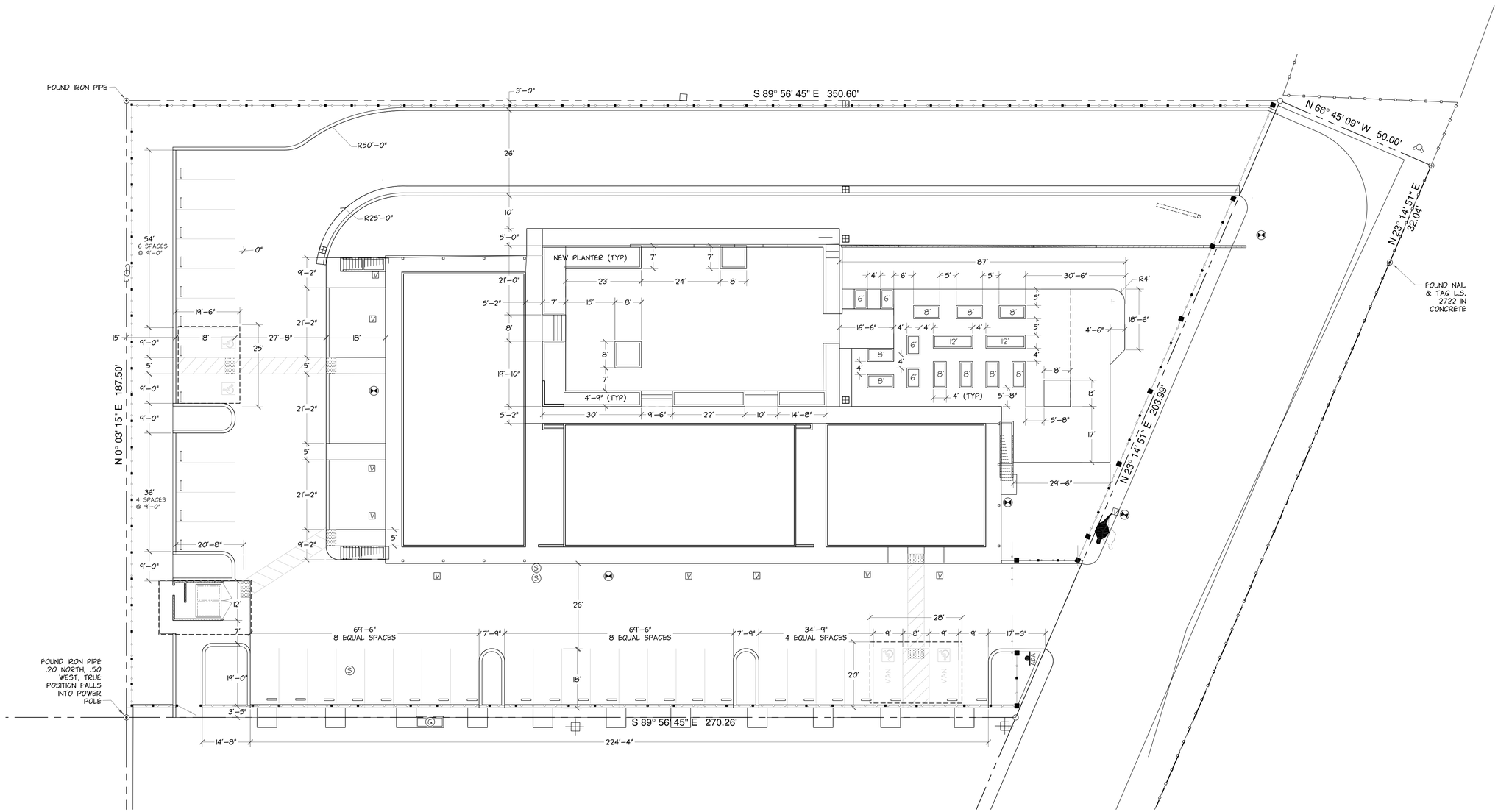
DESERT HAVEN APARTMENTS
VICTORVILLE, CALIFORNIA

CONSTRUCTION DETAILS

6-11-18

DRAWING: :

L-9



NORTH

16 8 0 16
SCALE: 1"=16'
CAUTION: GRAPHIC SCALE MUST BE USED EXCEPT WHERE DIMENSIONED

NO.	DATE	ISSUED FOR	BY
1	4-17-18	FOR REVIEW	

NOTE
0 1/2 1
BAR SHOULD MEASURE 1" OR DRAWING IS NOT TO SCALE

SCALE:
DESIGNED:
DRAWN: JRS
CHECKED:
APPROVED:

LAST EDIT DATE:
CAD REF. FILE:

Bisnett Design Associates
bda
16046 Baur Court
Guerneville, CA 95446
530.277.3703
brian@bisnettdesign.com
Landscape Architecture & Environmental Planning

DESERT HAVEN APARTMENTS
VICTORVILLE, CALIFORNIA

HORIZONTAL CONTROL PLAN

6-11-18

DRAWING::

L-1.0

DETENTION BASIN OVERFLOW OUTLET –SEE DETAIL L-9

DETENTION BASIN OVERFLOW

DROP INLET SEE DETAIL L-9

3" STYRENE PIPE FROM PLANTER TO DETENTION BASIN (TYPICAL)

4.6% SLOPE

CONCRETE CURB AND GUTTER

FEATHER PARKING LOT PAVING INTO ROAD. PAVING AS REQUIRED

HIGH-DENSITY POLYETHYLENE (HDPE) DUAL WALL w/ SMOOTH INTERIOR SIZE PER HYDRAULIC CALCULATIONS BELOW (TYPICAL)

ADA COMPLIANT PATIO, SLOPE NTE 1.9% IN ANY DIRECTION

DETENTION BASIN (E) POOL

4 RISERS @ .52 EA.

2 RISERS @ .43 EA.

3 RISERS @ .48 EA.

ADA COMPLIANT AREA, SLOPE NTE 1.9% IN ANY DIRECTION

RESURFACE AC @ ADA PARKING AREA AS REQUIRED

ADA COMPLIANT PARKING AREA, SLOPE NTE 1.9% IN ANY DIRECTION

4.1% SLOPE

6' LANDING 12' RAMP NTE 8.33% SLOPE RAMP w/ RAIL

ADA COMPLIANT LANDING, MIN. 5' WIDE, NTE 1.9% IN ANY DIRECTION (TYP)

PROVIDE FLUSH CURB ALONG WEST EDGE OF PARKING LOT

STRIP DRAINS (TYPICAL) DRAIN INTO DETENTION BASIN. SLOPE NEW SIDEWALK 2% TO STRIP DRAINS –SEE DETAIL L-7

DRAIN ALL DOWN SPOUTS IN COURTYARD TO DETENTION BASIN

PROVIDE NEW DOWNSPOUTS WHERE SHOWN. MIN 2"Dx2.5"W

RESURFACE AC @ ADA PATH OF TRAVEL TO TRASH ENCLOSURE AND SURROUNDING AREA AS REQUIRED

2748.00 CONTROL TOP OF SEWER

RAISE ELEVATION OF VAULTS AS REQ'D

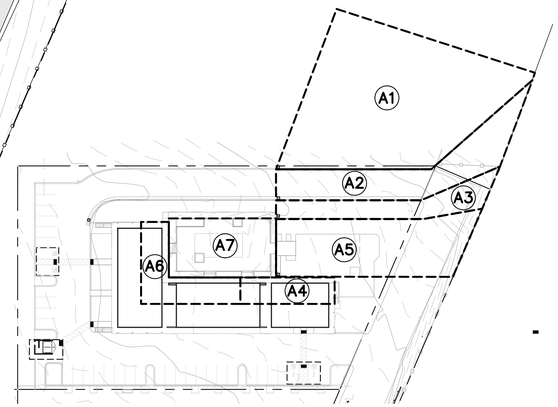
FEATHER PARKING LOT PAVING INTO ROAD PAVING AS REQUIRED

PROVIDE FLUSH CURB ALONG WEST EDGE OF PARKING LOT

MATCH EXISTING ASPHALT

MATCH EXISTING

D.S.



NOTE
BAR SHOULD MEASURE 1" OR DRAWING IS NOT TO SCALE
0 1/2 1

Zone	Subarea (acres)	Runoff Coefficient	Tc (minutes)	rainfall intensity (inches per hour)	Peak runoff (cfs)	drain pipe segment	load (cfs)	slope	(corrugated PE w/ smooth interior)
A1	0.42	0.2	6.5	0.48	0.04	C1	0.04	3.8%	4"
A2	0.10	0.9	6.5	0.48	0.04	C2	0.08	3.1%	4"
A3	0.08	0.4	9.5	0.36	0.01	C3	0.09	9.6%	4"
A4	0.08	1.0	5.0	0.68	0.06	C4	0.06	0.5%	4"
A5	0.14	0.6	7.5	0.43	0.04	C5	0.09	0.2%	4"
						C6	0.19	0.2%	6"
A6	0.12	1.0	5.0	0.68	0.08	C7	0.08	0.2%	4"
A7	0.11	1.0	5.0	0.68	0.08	C8	0.35	1.5%	6"

NORTH

16 8 0 16
SCALE: 1"=16'
CAUTION: GRAPHIC SCALE MUST BE USED EXCEPT WHERE DIMENSIONED

CITY OF VICTORVILLE GRADING IMPROVEMENTS GENERAL NOTES

1. Grading shall conform to Article 2 of Chapter 5 of Title 16 of the City of Victorville Municipal Code.
2. A Grading Permit shall be obtained from the City of Victorville Building Department prior to the start of grading work.
3. Contractor shall give the City of Victorville Building Department at least 2 working days notice to schedule a pre-construction meeting with the inspector prior to the start of work.
4. The contractor shall notify Underground Service Alert at 1 (800) 422-4133 at least 2 working days in advance of starting work.
5. It shall be the contractor's responsibility to familarize himself/herself with site conditions and of any underground utilities shown or not shown on these plans.
6. The contractor shall give the City of Victorville Building Department at least 1 working day notice prior to all inspections at (760) 955-5103. A re-inspection fee will be rendered on each occasion when the contractor is not ready for the inspection at the scheduled time. No further inspections will be performed until said re-inspection fee is paid.
7. It shall be the contractor's responsibility to protect survey monuments in place and the contractor shall be financially responsible for resetting damaged or destroyed monuments.
8. No grading shall commence prior to 7:00 a.m. each working day.
9. Dust control shall include:
a. Provisions for continuous watering during grading operations including a 24-hour on call operator or as directed by City Staff.
b. Upon completion of the project, the entire site is to be stabilized by treating with magnesium chloride or other approved method and as approved by City Staff
10. Clear all cut and fill areas of vegetation and organic materials to a depth of six (6) inches or more.
11. Fill shall be placed in lifts of eight (8) inches or less and sufficient moisture added and material compacted to acheve the required percent of compaction.
12. No rocks greater than six (6) inches in diameter may be placed in fill.
13. Compaction shall be a minimum of ninety percent (90%).
14. Minimum slope for all graded areas shall be one percent (1.00%) unless on smooth PCC, then the minimum slope shall be one-half percent (0.50%).
15. Maximum slopes shall be 2:1. Slope stabilization shall be provided on all slopes in accordance with Article 2 of Chapter 5 of Title 16 of the City of Victorville Municipal Code.
16. All off-site improvements shall be constructed to City of Victorville Standards
17. All fences and walls are under separate permit.
18. Retaining walls shall be constructed to City of Victorville Standards or wall calculations submitted to the Building Department for City approval.
19. Building pad and grading plan certification shall be completed by a licensed engineer in accordance with Article 2 of Chapter 5 of Title 16 of the City of Victorville Municipal Code.
20. The maximum allowable foundation bearing pressure shall be 1500 psi unless approved by the City of Victorville Building Department.
21. The contractor shall comply with the Grading Ordinance Section 16-5.02.060 and Section 16-5.02.070 of the City of Victorville Municipal Code as it relates to borrow pits, export sites and haul routes prior to issuance of a grading permit.
22. A Paleontological Monitor must be present during all phases of grading when required by City Staff.
23. Approximate Quantities:

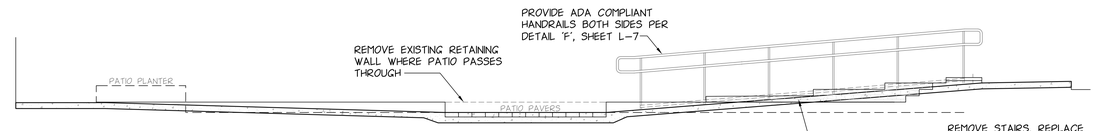
Cut 30.5 cu. yds.

Fill _____ cu. yds.

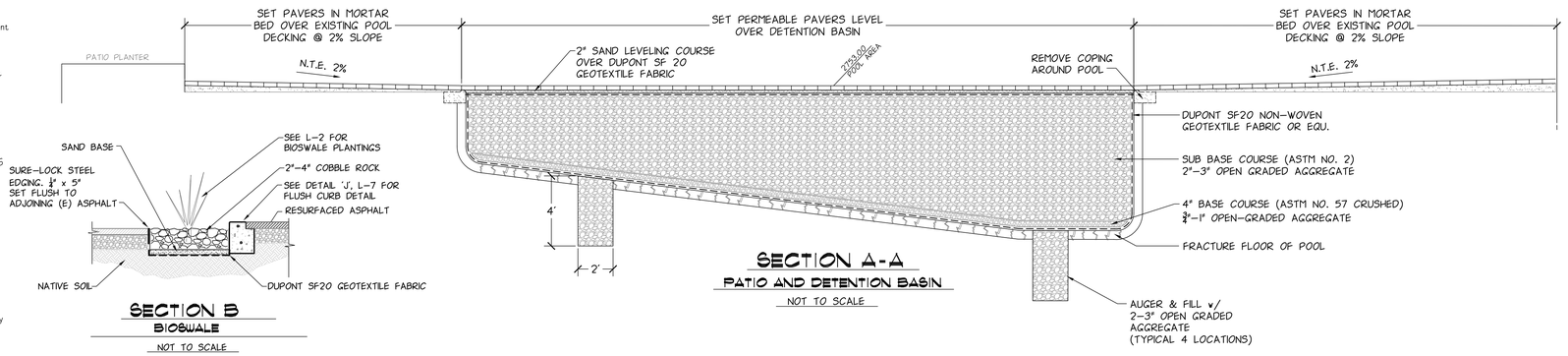
Shrinkage _____ %

24. Construction activities of one (1) acre or more shall require a General Construction Storm Water Permit. For more information, you may call the State Water Resources Control Board –Division of Water Quality at (916) 657-1146.

HYDRAULIC CALCULATIONS



SECTION C-C
RETAINING WALL
NOT TO SCALE



SECTION A-A
PATIO AND DETENTION BASIN
NOT TO SCALE

SECTION B
BIOSWALE
NOT TO SCALE

NO.	DATE	ISSUED FOR	BY
1	4-17-18	FOR REVIEW	

SCALE:
DESIGNED: JRS
DRAWN:
CHECKED:
APPROVED:
CAD REF. FILE:
LAST EDIT DATE:

Bisnett Design Associates
bda
16048 Bear Court
Victorville, CA 92394
(562)773-9793
brian@bisnettdesign.com
Landscape Architecture & Environmental Planning

DESERT HAVEN APARTMENTS
VICTORVILLE, CALIFORNIA

GRADING & DRAINAGE PLAN

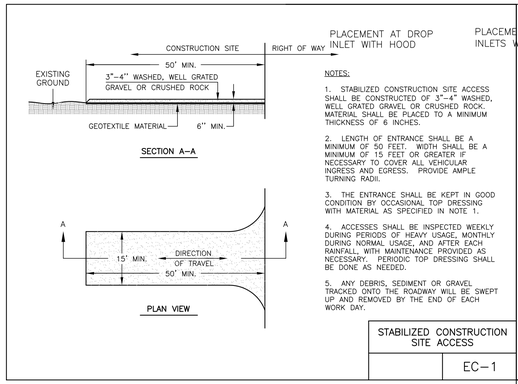
6-11-18

DRAWING:

L-11

EROSION CONTROL NOTES:

1. A STAND-BY CREW FOR EMERGENCY WORK SHALL BE AVAILIBLE AT ALL TIMES DURING THE RAINY SEASON (NOVEMBER 1 THRU APRIL 15). NECESSARY MATERIALS SHALL BE AVAILIBLE ON-SITE AND STOCKPILED AT CONVENIENT LOCATIONS TO FACILITATE RAPID CONSTRUCTION OF EMERGENCY DEVICES WHEN RAIN IS IMMINENT.
2. EROSION CONTROL DEVICES SHOWN ON THIS PLAN MAY BE REMOVED WHEN APPROVED BY THE BUILDING OFFICIAL IF THE GRADING OPERATION HAS PROGRESSED TO THE POINT WHERE THEY ARE NO LONGER REQUIRED.
3. GRADED AREAS ADJACENT TO FILL SLOPES LOCATED AT THE SITE PERIMETER MUST DRAIN AWAY FROM THE TOP OF SLOPE AT THE CONCLUSION OF EACH WORKING DAY. ALL LOOSE SOILS AND DEBRIS THAT MAY CREATE A POTENTIAL IMPACT TO OFF-SITE PROPERTY SHALL BE STABILIZED OR REMOVED FROM THE SITE ON A DAILY BASIS. DO NOT HOSE DOWN OR SWEEP SOIL OR SEDIMENT ACCUMULATED ON PAVEMENT OR OTHER IMPERVIOUS SURFACES INTO ANY STORMWATER CONVEYANCE (UNLESS CONNECTED TO A SEDIMENT BASIN, SEDIMENT TRAP, OR SIMILARLY EFFECTIVE CONTROL), STORM DRAIN INLET, OR SURFACE WATER.
4. ALL SILT AND DEBRIS SHALL BE REMOVED FROM ALL DEVICES WITHIN 24 HOURS AFTER EACH RAINSTORM AND BE DISPOSED OF PROPERLY.
5. A GUARD SHALL BE POSTED ON THE SITE WHENEVER THE DEPTH OF WATER IN ANY DEVICE EXCEEDS TWO FEET. THE DEVICE SHALL BE DRAINED OR PUMPED DRY WITHIN 24 HOURS AFTER EACH RAINSTORM. PUMPING AND DRAINING OF ALL BASINS AND DRAINAGE DEVICES MUST COMPLY WITH THE APPROPRIATE BMP OF RALL DEWATERING OPERATIONS.
6. THE PLACEMENT OF ADDITIONAL DEVICES TO REDUCE EROSION DAMAGE AND CONTAIN POLLUTANTS WITHIN THE SITE IS LEFT TO DISCRETION OF THE FIELD ENGINEER. ADDITIONAL DEVICES AS NEEDED SHALL BE INSTALLED TO RETAIN SEDIMENTS AND OTHER POLLUTANTS ON SITE.
7. DESILTING BASINS MAY NOT BE REMOVED OR MADE INOPERABLE BETWEEN NOVEMBER 1 AND APRIL 15 OF THE FOLLOWING YEAR WITHOUT THE APPROVAL OF THE BUILDING OFFICIAL.
8. STORM WATER POLLUTION AND EROSION CONTROL DEVICES ARE TO BE MODIFIED, AS NEEDED, AS THE PROJECT PROGRESSES, THE DESIGN AND PLACEMENT OF THESE DEVICES IS THE RESPONSIBILITY OF THE FIELD ENGINEER. PLANS REPRESENTING CHANGES MUST BE SUBMITTED FOR APPROVAL IF REQUESTED BY THE BUILDING OFFICIAL.
9. EVERY EFFORT SHOULD BE MADE TO ELIMINATE THE DISCHARGE OF THE NON-STORM WATER FROM THE PROJECT SITE AT ALL TIMES.
10. ERODED SEDIMENTS AND OTHER POLLUTANTS MUST BE RETAINED ON-SITE AND MAY NOT BE TRANSPORTED FROM THE SITE VIA SHEET FLOW, SWALES, AREA DRAINS, NATURAL DRAINAGE COURSES, OR WIND.
11. STOCKPILE OR EARTH AND OTHER CONSTRUCTION-RELATED MATERIALS MUST BE PROTECTED FROM BEING TRANSPORTED FROM THE SITE BY FORGES OF WIND OR WATER
12. FUELS, OILS, SOLVENTS, AND OTHER TOXIC MATERIALS MUST BE STORED IN ACCORDANCE WITH THEIR LISTED AND ARE NOT TO CONTAMINATE THE SOILS AND SURFACE WATERS. ALL APPROVED STORAGE CONTAINERS ARE TO BE PROTECTED FROM THE WEATHER. SPILLS MUST BE CLEANED UP IMEDIATELY AND DISPOSED OF IN A PROPER MANNER. SPILLS MAY NOT BE WASHED INTO THE DRAINAGE SYSTEM.
13. EXCESS OR WASTE CONCRETE MAY NOT BE WASHED INTO THE PUBLIC WAY OR ANY OTHER DRAINAGE SYSTEM. PROVISIONS SHALL BE MADE TO RETAIN CONCRETE WASTE ON-SITE UNTIL THEY CAN BE DISPOSED OF A SOLID WASTE.
14. DEVELOPER/CONTRACTORS ARE RESPONSIBLE TO INSPECT ALL EROSION CONTROL DEVICES AND BMPs ARE INSTALLED AND FUNCTIONING PROPERTY IF THERE IS A 40% ACTUAL PRECIPITATION. A CONSTRUCTION SITE INSPECTION CHECKLIST AND ACTUAL PRECIPITATION, AND AFTER ACTUAL PRECIPITATION. A CONSTRUCTION SITE INSPECTION CHECKLIST AND INSPECTION LOG SHALL BE MAINTAINED AT THE PROJECT SITE AT ALL TIMES AND AVAILIBLE FOR REVIEW BY THE BUILDING OFFICIAL (COPIES OF THE SELF-INSPECTION CHECK LIST AND INSPECTION LOGS ARE AVAILIBLE UPON REQUEST).
15. SEDIMENTS AND OTHER MATERIALS MAY NOT BE TRACKED FROM THE SITE BY VEHICLE TRAFFIC. THE CONSTRUCTION ENTRANCE ROADWAYS MUST BE STABILIZED SO AS TO INHIBIT SEDIMENT FROM BEING DEPOSITED INTO THE PUBLIC WAY. ACCIDENTAIL DEPOSITION MUST BE SWEPT UP IMMEDIATELY AND MAY NOT BE WASHED DOWN BY RAIN OR OTHER MEANS.
16. ANY SLOPES WITH DISTURBED SOILS OR DENUDED OF VEGETATION MUST BE STABILIZED SO AS TO INHIBIT EROSION BY WIND AND WATER.
17. EROSION CONTROL FACILITIES AND MEASURES ARE TO BE INSTALLED AND OPERABLE AT ALL TIMES DURING CONSTRUCTION.
18. CHANGES TO THE EROSION CONTROL MEASURES INDICATED ON THESE PLANS AND DESCRIBED HEREIN TO ACCOMMODATE FIELD CONDITIONS MAY BE MADE ONLY WITH THE PRIOR APPROVAL OF, OR AT THE DIRECTION OF THE STATE REPRESENTATIVE.
19. EARTHEN BERMS AND STRAW BALE DIKES OR GEOTEXTILE FABRIC BARRIER SHALL BE CONSTRUCTED TO PREVENT OFF-FLOW OR SILTATION FROM THE PROJECT SITE. THE BERMS AND DIKES SHALL BE MAINTAINED IN PLACE UNTIL THE CONCLUSION OF THE SITE PAVING.
20. ALL PAVED AREAS SHALL BE KEPT CLEAR OF EARTH MATERIAL AND DEBRIS THE SITE SHALL BE MAINTAINED SO AS TO PREVENT SEDIMENT- LADEN RUNOFF FROM ENTERING THE STORM DRAINAGE SYSTEM OR ADJACENT PROPERTIES.
21. ALL EROSION CONTROL FACILITIES SHALL BE INSPECTED BY THE CONTRACTOR AND REPAIRED AS REQUIRED AT THE CONCLUSION OF EACH WORKING DAY DURING THE RAINY SEASON THE CONTRACTOR SHALL INSPECT THE EROSION CONTROL FACILITIES AND MAKE NECESSARY REPAIRS THERETO PRIOR TO ANTICIPATED STORMS AND SHALL PERIODICALLY INSPECT THE SITE AT REASONABLE INTERVALS DURING STORMS OF EXTENDED DURATION. REPAIRS TO DAMAGED FACILITIES SHALL BE REPAIRED IMMEDIATELY.
22. THE CONTRACTOR SHALL BE SOLELY RESPONSIBLE FOR INSTALLATION AND MAINTENANCE OF THE EROSION CONTROL FACILITIES AND SHALL CONDUCT PERIODIC INSPECTION OF THE PROJECT SITE DURING STORMS OF PROLONGED DURATION AND/OR HEAVY INTENSITY TO ASSURE THAT THEY FUNCTION IN THE MANNER DESCRIBED HEREIN.
23. ALL LOOSE SOIL AND DEBRIS SHALL BE REMOVED FROM THE STREET AREAS UPON STARTING OPERATIONS AND AT THE END OF EACH WORKDAY OR AS NECESSARY.
24. THE CONTRACTOR SHALL INSTALL THE STABILIZED CONSTRUCTION ENTRANCE PRIOR TO COMMENCEMENT OF GRADING. LOCATION OF THE ENTRANCE MAY BE ADJUSTED BY THE CONTRACTOR TO FACILITATE GRADING OPERATIONS. ALL CONSTRUCTION TRAFFIC ENTERING THE PAVED ROAD MUST CROSS THE STABILIZED CONSTRUCTION ENTRANCE. THE STABILIZED CONSTRUCTION ENTRANCE SHALL REMAIN IN PLACE UNTIL THE ROAD BASE ROCK COURSE IS COMPLETED.
25. AS STORM DRAIN IMPROVEMENTS ARE CONSTRUCTED, ALL STRUCTURES AND INLET PIPES SHALL BE PROTECTED FROM INFLOW OR SILT WITH FILTER BAGS OR GRAVEL BAG SILT BARRIERS.
26. ADJACENT PROPERTIES SHALL BE PROTECTED FROM STORM WATERS, MUD, SILT, ETC. ON A DAILY BASIS.
27. DUST CONTROL SHALL BE MAINTAINED AT ALL TIMES DURING CONSTRUCTION AND UNTIL FINAL COMPLETION. THE CONTRACTOR WHEN HE OR HIS SUBCONTRACTOR ARE OPERATING EQUIPMENT ON- SITE, SHALL PREVENT THE FORMATION OF ANY AIRBORNE NUISANCE BY WATERING AND/OR TREATING THE SITE OF THE WORK IN SUCH A MANNER THAT WILL CONFINE DUST PARTICLES TO THE IMMEDIATE SURFACE OF THE WORK. ADDITIONAL WATERING SHALL BE PROVIDED ON DRY OR WINDY DAYS. THE CONTRACTOR WILL BE RESPONSIBLE FOR ANY DAMAGE CAUSED BY DUST FROM HIS OWN ACTIVITIES OR HIS SUBCONTRACTORS ACTIVITIES IN PERFORMING THE WORK UNDER THIS CONTRACT AND SHALL BE RESPONSIBLE FOR ANY CITATIONS, FINES, OR CHARGES RESULTING FROM DUST NUISANCE. DUST CONTROL WILL BE DONE ON A DAILY BASIS.
28. MAINTENANCE IS TO BE PERFORMED AS FOLLOWS:
    - A. THE EROSION AND SEDIMENT CONTROL MEASURES SHALL BE INSPECTED EVERYDAY AND AFTER EACH STORM.
    - B. SILT FENCES, BERMS AND SWALES ARE TO BE INSPECTED AFTER EACH STORM AND REPAIRS MADE AS NEEDED. GRAVEL BAGS PLACED AROUND CURB INLETS SHALL BE INSPECTED AND REPLACED IF DAMAGED.
    - C. SEDIMENT SHALL BE REMOVED AND SEDIMENT TRAPS RESTORED TO ORIGINAL DIMENSIONS WHEN SEDIMENT HAS ACCUMULATED TO WITHIN A FOOT OF OUTLET ELEVATION OR TO 1/2 THE HEIGHT OF ANY PERIMETER CONTROL.
    - D. SEDIMENT REMOVED FROM TRAPS SHALL BE DEPOSITED IN A SUITABLE AREA AND IN SUCH A MANNER THAT IT WILL NOT ERODE. E. SEEDED AREAS WILL BE REPAIRED, RESEEDED AND MULCHED AS SOON AS POSSIBLE AFTER DAMAGED.
29. CONTRACTOR SHALL IMPLEMENT HOUSEKEEPING PRACTICES AS FOLLOWS:
    - A. SOLID WASTE MANAGEMENT: PROVIDE DESIGNATED WASTE COLLECTION AREAS AND CONTAINERS, ARRANGE FOR REGULAR REMOVAL AND DISPOSAL. CLEAR SITE OF TRASH INCLUDING ORGANIC DEBRIS, PACKAGING MATERIALS, SCRAP OR SURPLUS BUILDING MATERIALS AND DOMESTIC WASTE DAILY.
    - B. MATERIAL DELIVERY AND STORAGE: PROVIDE A DESIGNATED MATERIAL STORAGE AREA WITH SECONDARY CONTAINMENT SUCH AS BERMING. STORE MATERIAL ON PALLETS AND PROVIDE COVERING FOR SOLUBLE MATERIALS. RELOCATE STORAGE AREA INTO BUILDING SHELL WHEN POSSIBLE. INSPECT AREA WEEKLY.
    - C. CONCRETE WASTE: PROVIDE A DESIGNATED CONCRETE TRUCK WASHOUT BIN. DISPOSE OF HARDENED CONCRETE OFFSITE. AT NO TIME SHALL A CONCRETE TRUCK DUMP ITS WASTE AND CLEAN ITS TRUCK INTO THE CITY STORM DRAINS VIA CURB AND GUTTER. INSPECT DAILY TO CONTROL RUNOFF, AND WEEKLY FOR REMOVAL OF HARDENED CONCRETE.
    - D. PAINT AND PAINTING SUPPLIES: PROVIDE INSTRUCTION TO EMPLOYEES AND SUBCONTRACTORS REGARDING REDUCTION OF POLLUTANTS INCLUDING MATERIAL STORAGE, USE, AND CLEAN UP. INSPECT SITE WEEKLY FOR EVIDENCE OF IMPROPER DISPOSAL.
    - E. VEHICLE FUELING, MAINTENANCE AND CLEANING: PROVIDE A DESIGNATED FUELING AREA WITH SECONDARY CONTAINMENT SUCH AS BERMING. DO NOT ALLOW MOBILE FUELING OF EQUIPMENT. PROVIDE EQUIPMENT WITH DRIP PANS. RESTRICT ONSITE MAINTENANCE AND CLEANING OF EQUIPMENT TO A MINIMUM. INSPECT AREA WEEKLY.
    - F. PORTABLE TOILETS SHALL BE LOCATED AWAY FROM ALL STORM DRAIN INLETS.
    - G. GRADING OPERATIONS AND CONSTRUCTION SHALL BECONDUCTED IN A MANNER AND/OR MEASURES TAKEN TO PREVENT SAND, DUST, AND DEBRIS FROM BEING BLOWN ONTO OTHER PROPERTIES. AN ADEQUATE DUST PALLIATIVE SHALL BE USED AT ALL TIMES. AFTER COMPLETION OF GRADING THE DEVELOPER SHALL MAINTAIN THE SITE SUCH THAT SAND, DUST AND DEBRIS DO NOT BLOW ONTO OTHER PROPERTIES.
    - H. TEMPORARY FENCING SHALL BE ERECTED AS REQUIRED BY CITY STAFF DURING CONSTRUCTION TO PREVENT WINDBLOWN DEBRIS FROM LEAVING THE PROJECT SITE AND TO ENSURE PUBLIC SAFETY.



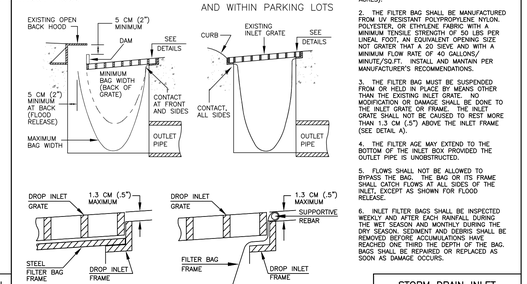
PLACEMENT AT DROP INLET WITH HOOD

NOTES:

1. STABILIZED CONSTRUCTION SITE ACCESS SHALL BE CONSTRUCTED OF 3"-4" WASHED, WELL GRATED GRAVEL OR CRUSHED ROCK. MATERIAL SHALL BE PLACED TO A MINIMUM THICKNESS OF 6 INCHES.
2. LENGTH OF ENTRANCE SHALL BE A MINIMUM OF 50 FEET. WIDTH SHALL BE A MINIMUM OF 15 FEET OR GREATER IF NECESSARY TO COVER ALL VEHICULAR INGRESS AND EGRESS. PROVIDE AMPLE TURNING RADI.
3. THE ENTRANCE SHALL BE KEPT IN GOOD CONDITION BY OCCASIONAL TOP DRESSING WITH MATERIAL AS SPECIFIED IN NOTE 1.
4. ACCESSES SHALL BE INSPECTED WEEKLY DURING PERIODS OF HEAVY USAGE, MONTHLY DURING NORMAL USAGE, AND AFTER EACH RAINFALL, WITH MAINTENANCE PROVIDED AS NECESSARY. PERIODIC TOP DRESSING SHALL BE DONE AS NEEDED.
5. ANY DEBRIS, SEDIMENT OR GRAVEL TRACKED ONTO THE ROADWAY WILL BE SWEPT UP AND REMOVED BY THE END OF EACH WORK DAY.

STABILIZED CONSTRUCTION SITE ACCESS	EC-1

PLACEMENT AT DROP INLETS WITHOUT HOOD AND WITHIN PARKING LOTS



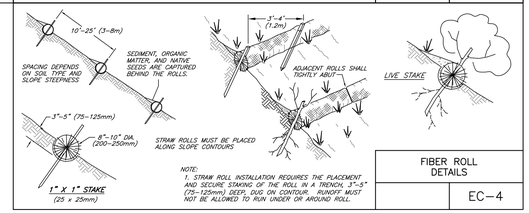
NOTES:

1. THE MAXIMUM DRAINAGE AREA PER FILTER SHALL BE NO MORE THAN 0.8 HECTARES (2 ACRES).
2. THE FILTER BAG SHALL BE MANUFACTURED FROM UV RESISTANT POLYPROPYLENE NYLON, POLYESTER, OR ETHYLENE FABRIC WITH A MINIMUM TENSILE STRENGTH OF 50 LBS PER LINEAL FOOT, AN EQUIVALENT OPENING SIZE NOT GRATER THAT A 20 SIEVE AND WITH A MINIMUM FLOW RATE OF 40 GALLONS/ MINUTE/SQ.FT. INSTALL AND MAINTAIN PER MANUFACTURER'S RECOMMENDATIONS.
3. THE FILTER BAG MUST BE SUSPENDED FROM OR HELD IN PLACE BY MEANS OTHER THAN THE EXISTING INLET GRATE. NO MODIFICATION OR DAMAGE SHALL BE DONE TO THE INLET GRATE OR FRAME. THE INLET GRATE SHALL NOT BE CAUSED TO REST MORE THAN 1.3 CM (.5") ABOVE THE INLET FRAME (SEE DETAIL A).
4. THE FILTER AGE MAY EXTEND TO THE BOTTOM OF THE INLET BOX PROVIDED THE OUTLET PIPE IS UNOBSTRUCTED.
5. FLOWS SHALL NOT BE ALLOWED TO BYPASS THE BAG. THE BAG OR ITS FRAME SHALL CATCH FLOWS AT ALL SIDES OF THE INLET, EXCEPT AS SHOWN FOR FLOOD RELEASE.
6. INLET FILTER BAGS SHALL BE INSPECTED WEEKLY AND AFTER EACH RAINFALL DURING THE WET SEASON AND MONTHLY DURING THE DRY SEASON. SEDIMENT AND DEBRIS SHALL BE REMOVED BEFORE ACCUMULATIONS HAVE REACHED ONE THIRD THE DEPTH OF THE BAG. BAGS SHALL BE REPAIRED OR REPLACED AS SOON AS DAMAGE OCCURS.

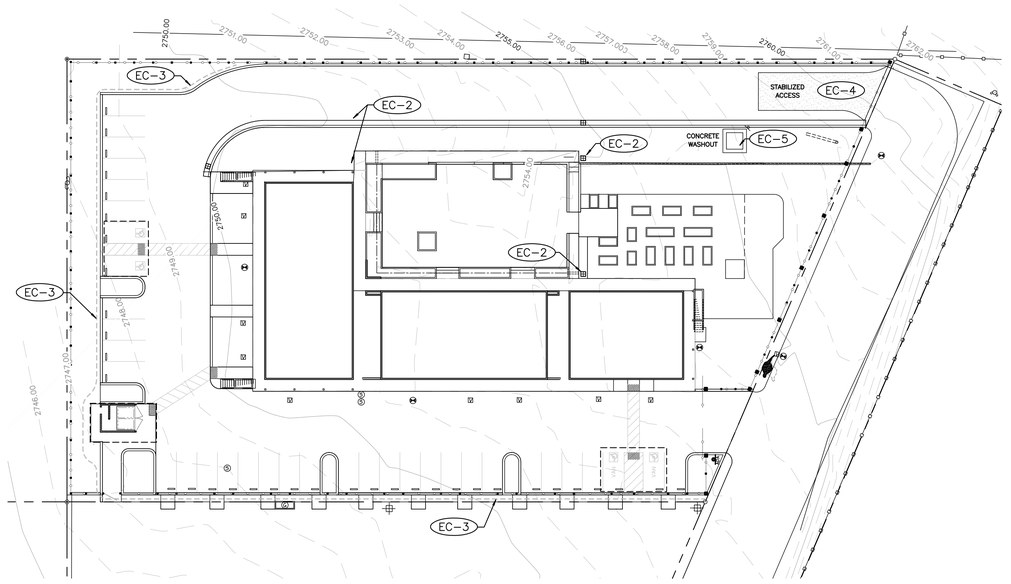
STORM DRAIN INLET FILTER BAG	EC-2

EROSION CONTROL KEY:

- EC-1 INSTALL FILTERBAG IN EXISTING CATCH BASIN DURING CONSTRUCTION.
- EC-2 INSTALL FIBER ROLLS OR GRAVEL BAGS AROUND AND FILTERBAGS WITHIN DROP INLETS DURING CONSTRUCTION AND INSTALL GRAVEL BAGS AROUND MANHOLES DURING CONSTRUCTION.
- EC-3 CONSTRUCT FIBER ROLLS ALONG FRONTAGES, ALONG STREETS, OFF BACK OF WALK AND PROPERTY LINES WHERE CROSS-DRAINAGE IS POSSIBLE, AND SURROUNDING ALL STOCKPILED SEDIMENT OR SOIL.
- EC-4 CONSTRUCT STABILIZED CONSTRUCTION ENTRANCE. MOVE ENTRANCE AS NEEDED TO ALLOW ROOM FOR CONSTRUCTION.
- EC-5 RENT CONCRETE WASHOUT BIN AND PLACE IN A LOCATION ACCESSIBLY TO CONCRETE TRUCKS.



FIBER ROLL DETAILS	EC-4



NORTH

30 15 0 30
SCALE: 1"=30'
CAUTION: GRAPHIC SCALE MUST BE USED EXCEPT WHERE DIMENSIONED

NO.	DATE	ISSUED FOR	BY
1	4-17-18	FOR REVIEW	

NOTE
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BAR SHOULD MEASURE 1" OR DRAWING IS NOT TO SCALE

LAST EDIT DATE:

SCALE:
DESIGNED:
DRAWN: JCS
CHECKED:
APPROVED:
CAD REF. FILE:

Bisnett Design Associates
bda
16648 Bear Court
Grass Valley, CA 95949
530.277.9733
brian@bisnettdesign.com
Landscape Architecture & Environmental Planning

DESERT HAVEN APARTMENTS
VICTORVILLE, CALIFORNIA

EROSION CONTROL PLAN

6-11-18

DRAWING: :

L-12

RECOMMENDED PERFORMANCE GUIDELINE FOR EMULSIFIED ASPHALT SLURRY SEAL

1. SCOPE

The intent of this guideline is to aid in the design, testing, quality control, measurement and payment procedures for the application of Emulsified Asphalt Slurry Seal Surfacing.

2. DESCRIPTION

Slurry seal shall consist of a mixture of an emulsified asphalt, mineral aggregate, water, and additives, proportioned, mixed and uniformly spread over a properly prepared surface as directed by the Buyer's Authorized Representative (B.A.R.). The slurry seal shall be applied as a homogeneous mat, adhere firmly to the prepared surface, and have a skid-resistant texture throughout its service life.

3. SPECIFICATIONS

It is not normally required to run all tests on every project. A compilation of results from the listed tests should be indicative of system performance. Failure to meet specification for an individual test does not necessarily disqualify the system. If, for example, the system to be used on the project has a record of good performance, individual requirements for testing may be waived. Agency and testing methods are listed in the appendix (see Appendix A) and form a part of this guideline.

4. MATERIALS

4.1 EMULSIFIED ASPHALT

The emulsified asphalt, and emulsified asphalt residue, shall meet the requirements of AASHTO M 140 or ASTM D 977 for SS-1 or SS-1h. For CSS-1, CSS-1h, or CQS-1h, it shall meet the requirements of AASHTO M 208 or ASTM D 2397.
Each load of emulsified asphalt shall be accompanied with a Certificate of Analysis/Compliance to indicate that the emulsion meets the specifications.

4.2 AGGREGATE

4.2.1 GENERAL

The mineral aggregate used shall be the type specified for the particular application requirements of the slurry seal. The aggregate shall be crushed stone such as granite, slag, limestone, chat, or other high-quality aggregate, or combination thereof. To assure the material is 100 percent crushed, the parent aggregate will be larger than the largest stone in the gradation to be used.

4.2.2 QUALITY TESTS

The aggregate should meet agency specified polishing values and these minimum requirements:

TEST	TEST METHOD AASHTO	TEST METHOD ASTM	SPECIFICATION
Sand Equivalent Value of Soils and Fine Aggregate	T 176	D 2419	45 Minimum
Soundness of Aggregates by Use of Sodium Sulfate or Magnesium Sulfate	T 104	C 88	15% Maximum w/NA_2SO_4 25% Maximum w/$MgSO_4$
Resistance to Degradation of Small-Size Coarse Aggregate by Abrasion and Impact in the Los Angeles Machine[1]	T 96	C 131	35% Maximum

[1]The abrasion test is run on the parent aggregate.

4.2.3 GRADATION

When tested in accordance with AASHTO T 27 (ASTM C 136) and AASHTO T 11 (ASTM C 117), the mix design aggregate gradation shall be within one of the following bands (or one recognized by the local paving authority):

SIEVE SIZE	TYPE I PERCENT PASSING	TYPE II PERCENT PASSING	TYPE III PERCENT PASSING	STOCKPILE TOLERANCE FROM THE MIX DESIGN GRADATION
3/8 (9.5 mm)	100	100	100	
# 4 (4.75 mm)	100	90 - 100	70 - 90	± 5%
# 8 (2.36 mm)	90 - 100	65 - 90	45 - 70	± 5%
# 16 (1.18 mm)	65 - 90	45 - 70	28 - 50	± 5%
# 30 (600 um)	40 - 65	30 - 50	19 - 34	± 5%
# 50 (330 um)	25 - 42	18 - 30	12 - 25	± 4%
#100 (150 um)	15 - 30	10 - 21	7 - 18	± 3%
#200 (75 um)	10 - 20	5 - 15	5 - 15	± 2%

The gradation of the aggregate stockpile shall not vary by more than the stockpile tolerance from the mix design gradation (indicated in the table above) while also remaining within the specification gradation band. The percentage of aggregate passing any two successive sieves shall not change from one end of the specified range to the other end.

The aggregate will be accepted at the job location or stockpile based on five gradation tests sampled according to AASHTO T 2 (ASTM D 75). If the average of the five tests is within the stockpile tolerance from the mix design gradation, the material will be accepted. If the average of those test results is out of specification or tolerance, the contractor will be given the choice to either remove the material or blend additional

aggregate with the stockpile material to bring it into compliance. Materials used in blending must meet the required aggregate quality test specifications in Section 4.2.2 before blending and must be blended in a manner to produce a consistent gradation. Aggregate blending may require a new mix design.

Screening shall be required at the stockpile if there are any problems created by oversized materials in the mix.

Type I. This aggregate gradation is used to fill surface voids, address moderate surface distresses, and provide protection from the elements. The fineness of this mixture provides the ability for some crack penetration.

Type II. This aggregate gradation is used to fill surface voids, address more severe surface distresses, seal, and provide a durable wearing surface.

Type III. This aggregate gradation provides maximum skid resistance and an improved wearing surface.

4.3 MINERAL FILLER

Mineral filler may be used to improve mixture consistency and to adjust mixture breaking and curing properties. Portland cement, hydrated lime, limestone dust, fly ash, or other approved filler meeting the requirements of ASTM D 242 shall be used if required by the mix design. Typical use levels are normally 0.0 - 3.0 percent and may be considered part of the aggregate gradation.

4.4 WATER

The water shall be free of harmful salts and contaminants. If the quality of the water is in question, it should be submitted to the laboratory with the other raw materials for the mix design.

4.5 ADDITIVES

Additives may be used to accelerate or retard the break/set of the slurry seal. Appropriate additives, and their applicable use range, should be approved by the laboratory as part of the mix design.

5. LABORATORY EVALUATION

5.1 GENERAL

Before work begins, the contractor shall submit a signed mix design covering the specific materials to be used on the project. This design will be performed by a laboratory which has experience in designing Emulsified Asphalt Slurry Seal Surfacing. After the mix design has been approved, no material substitution will be permitted unless approved by the B.A.R.

ISSA can provide a list of laboratories experienced in slurry seal design.

5.2 MIX DESIGN

Compatibility of the aggregate, emulsified asphalt, water, mineral filler and other additives shall be evaluated in the mix design. The mix design shall be completed using materials consistent with those supplied by the contractor for the project. Recommended tests and values are as follows:

TEST	ISSA TB NO.	SPECIFICATION
Mix Time @ 77°F (25°C)	TB 113	Controllable to 180 Seconds Minimum
Slurry Seal Consistency	TB 106	0.79 – 1.18 inches (2.0 – 3.0 cm)
Wet Cohesion @ 30 Minutes Minimum (Set) @ 60 Minutes Minimum (Traffic)	TB 139 (For quick-traffic systems)	12 kg-cm Minimum 20 kg-cm or Near Spin Minimum
Wet Stripping	TB 114	Pass (90% Minimum)
Wet-Track Abrasion Loss One-hour Soak	TB 100	75 g/ft² (807 g/m²) Maximum
Excess Asphalt by LWT Sand Adhesion	TB 109 (Critical in heavy-traffic areas)	50 g/ft² (538 g/m²) Maximum

The Wet Track Abrasion Test is performed under laboratory conditions as a component of the mix design process. The purpose of this test is to determine the minimum asphalt content required in a slurry seal system. The Wet Track Abrasion Test is not recommended as a field quality control or acceptance test. ISSA TB 136 describes potential causes for inconsistent results of the Wet Track Abrasion Test.

The mixing test is used to predict the time the material can be mixed before it begins to break. It can be a good reference check to verify consistent sources of material. The laboratory should verify that mix and set times are appropriate for the climatic conditions expected during the project.

The laboratory shall also report the quantitative effects of moisture content on the unit weight of the aggregate (bulking effect) according to AASHTO T19 (ASTM C29). The report must clearly show the proportions of aggregate, mineral filler (if used) and emulsified asphalt based on the dry weight of the aggregate.

The percentages of each individual material required shall be shown in the laboratory report. Based on field conditions, adjustments within the specific ranges of the mix design may be required.

The component materials shall be designed within the following limits:

COMPONENT MATERIALS	SUGGESTED LIMITS
Residual Asphalt	Type I: 10 - 16% Type II: 7.5 - 13.5% Type III: 6.5 - 12% (Based on dry weight of aggregate)
Mineral Filler	0.0 - 3.0% (Based on dry weight of aggregate)
Additives	As needed
Water	As required to produce proper mix consistency

5.3 MIX TOLERANCES

Tolerances for the slurry seal mixture are as follows:

a. After the residual asphalt content is determined, a variation ±1% by weight of dry aggregate will be permitted.
b. The slurry consistency, as determined according to ISSA TB No. 106, shall not vary more than ± 0.2" (± 0.5 cm) from the job mix formula after field adjustments.
c. The rate of application shall not vary more than ± 2 lb/yd² (± 1.1 kg/m²) when the surface texture does not vary significantly.

6. EQUIPMENT

6.1 GENERAL

All equipment, tools, and machines used in the application of slurry seal shall be maintained in satisfactory working condition at all times.

6.2 MIXING EQUIPMENT

The machine shall be specifically designed and manufactured to apply slurry seal. The material shall be mixed by an automatic-sequenced, self-propelled, slurry seal mixing machine of either truck-mounted or continuous-run design. Continuous-run machines are those that are equipped to self-load materials while continuing to apply slurry seal. Either type machine shall be able to accurately deliver and proportion the mix components through a mixer and to discharge the mixed product on a continuous-flow basis. Sufficient storage capacity for all mix components is required to maintain an adequate supply to the proportioning controls.

The B.A.R. should decide which type of equipment best suits the specific project. In some cases, truck-mounted machines may be more suited, i.e. cul-de-sacs, small narrow roadways, parking lots, etc. On some projects, continuous-run equipment may be chosen due to the continuity of mix and the reduction of start-up joints. Generally, truck-mounted machines or continuous-run machines may be used on similar projects.

If continuous-run equipment is used, the machine shall provide the operator with full control of the forward and reverse speeds during application of the slurry seal. It shall be equipped with a self-loading device and opposite-side driver stations. The self-loading device, opposite-side driver stations, and forward and reverse speed controls shall be of original-equipment-manufacturer design.

6.3 PROPORTIONING DEVICES

Individual volume or weight controls for proportioning mix components shall be provided and properly labeled. These proportioning devices are used in material calibration to determine the material output at any time.

6.4 SPREADING EQUIPMENT

The mixture shall be placed uniformly by means of a spreader box attached to the paver and mechanically equipped, if necessary, to agitate and spread the material evenly throughout the box. With some quick-set systems, mechanical agitation may extend mix time. The slurry seal mixture shall have the proper consistency as it enters the spreader box. Spraying of additional water into the spreader box will not be permitted.

A front seal shall be utilized to ensure no loss of the mixture at the road contact point. The rear seal shall act as final strike-off and shall be adjustable. The spreader box and rear seal shall be designed and operated to provide uniform mix consistency behind the box. The spreader box shall have suitable means to side-shift to compensate for variations in the pavement width. A burlap drag or other approved screed may be attached to the rear of the spreader box to provide a highly textured uniform surface. A drag stiffened by hardened slurry is ineffective and should be replaced immediately.

6.5 AUXILIARY EQUIPMENT

Suitable surface preparation equipment, traffic control equipment, hand tools, and other support and safety equipment necessary to perform the work shall be provided by the contractor.

7. CALIBRATION

Each mixing unit to be used in performance of the work shall be calibrated in the presence of the B.A.R. prior to the start of the project. Previous calibration documentation covering the exact materials to be used may be acceptable, provided the calibration was performed during the previous 60 days. The documentation shall include an individual calibration of each material at various settings, which can be related to the machine's metering devices. Any equipment replacement affecting material proportioning requires that the machine be recalibrated. No machine will be allowed to work on the project until the calibration has been accepted. ISSA Inspector's Manual describes a method of machine calibration. ISSA contractors and/or machine manufacturers may also provide methods of machine calibration.

8. WEATHER LIMITATIONS

The slurry seal shall not be applied if either the pavement or air temperature is below 50°F (10°C) and falling, but may be applied when both pavement and air temperatures are above 45°F (7°C) and rising. No slurry seal shall be applied when there is the possibility of freezing temperatures at the project location within 24 hours after application. The mixture shall not be applied when weather conditions prolong opening to traffic beyond a reasonable time.

9. NOTIFICATION AND TRAFFIC CONTROL

9.1 NOTIFICATION

Homeowners and businesses affected by the paving shall be notified at least one day in advance of the surfacing. Should work not occur on the specified day, a new notification will be distributed. The notification shall be posted in written form, stating the time and date that the surfacing will take place. If necessary, signage alerting traffic to the intended project should be posted.

9.2 TRAFFIC CONTROL

Traffic control devices shall be in accordance with agency requirements and, if necessary, conform to the requirements of the Manual on Uniform Traffic Control Devices. Opening to traffic does not constitute acceptance of the work.

In areas that are subject to an increased rate of sharp-turning vehicles, additional time may be required for a more complete cure of the slurry seal mat to prevent damage. Tire marks may be evident in these areas after opening but typically diminish over time with rolling traffic.

10. SURFACE PREPARATION

10.1 GENERAL

Prior to applying the slurry seal, loose material, oil spots, vegetation, and other objectionable material shall be removed. Any standard cleaning method will be acceptable. If water is used, cracks shall be allowed to dry thoroughly before slurry surfacing. Manholes, valve boxes, drop inlets and other service entrances shall be protected from the slurry seal by a suitable method. The B.A.R. shall approve the surface preparation prior to surfacing.

10.2 TACK COAT

Normally, tack coat is not required unless the surface to be covered is extremely dry and raveled or is concrete or brick. If required, the emulsified asphalt should be SS, CSS, or the slurry seal emulsion. Consult with the slurry seal emulsion supplier to determine dilution stability. The tack coat may consist of one part emulsified asphalt/three parts water and should be applied with a standard distributor. The distributor shall be capable of applying the dilution evenly at a rate of 0.05-0.15 gal/yd² (0.23-0.68 l/m²). The tack coat shall be allowed to cure sufficiently before the application of slurry seal. If a tack coat is to be required, it must be noted in the project plans.

10.3 CRACKS

It is recommended to treat cracks wider than 0.25" (0.64cm) in the pavement surface with an approved crack sealer prior to application of the slurry seal.

11. APPLICATION

11.1 GENERAL

If required, it is recommended that a test strip be placed in conditions similar to those expected to be encountered during the project.

The surface may be wetted with water ahead of the spreader box. The rate of application of the water spray shall be adjusted during the day to suit temperature, surface texture, humidity, and dryness of the pavement. Pooling or standing water shall be avoided.

The slurry seal shall be of the desired consistency upon exiting the mixer. A sufficient amount of material shall be carried in all parts of the spreader box at all times so that complete coverage is achieved. Overloading of the spreader shall be avoided.

No lumping, balling, or unmixed aggregate shall be permitted.

Significant streaks, such as those caused by oversized aggregate or broken mix, shall not be left in the finished surface. If excessive streaking occurs, the job will be stopped until the cause of the problem has been corrected. Some situations may require screening the aggregate prior to loading it into the units going from the stockpile area to the jobsite.

11.2 RATE OF APPLICATION

The slurry seal mixture shall be of the proper consistency at all times so as to provide the application rate required by the surface condition. The average application rate shall be in accordance with the following table:

AGGREGATE TYPE	LOCATION	SUGGESTED APPLICATION RATE
Type I	Parking Areas Urban and Residential Streets Airport Runways	8 - 12 lb/yd² (4.3 - 6.5 kg/m²)
Type II	Urban and Residential Streets Airport Runways	10 - 18 lb/yd² (5.4 - 9.8 kg/m²)
Type III	Primary and Interstate Routes	15 - 22 lb/yd² (8.1 - 12.0 kg/m²)

Suggested application rates are based upon the weight of dry aggregate in the mixture. Application rates are affected by the unit weight and gradation of the aggregate and the demand of the surface to which the slurry seal is being applied.

11.3 JOINTS

No excess buildup, uncovered areas, or unsightly appearance shall be permitted on longitudinal or transverse joints. The contractor shall provide suitable equipment to produce a minimum number of longitudinal joints throughout the project. When possible, a longitudinal joint shall not be placed in a wheel path. Less than full box width passes will be used only as required. If less than full box width passes are used, they shall not be the last pass of any paved area. A maximum of 6" (15.2 cm) shall be allowed for overlap of longitudinal joints.

11.4 MIXTURE

The slurry seal shall possess sufficient stability so that premature breaking of the material in the spreader box does not occur. The mixture shall be homogeneous during and following mixing and spreading. It shall be free of excess liquids which create segregation of the aggregate. Spraying of additional water into the spreader box will not be permitted.

11.5 HANDWORK

Areas which cannot be accessed by the mixing machine shall be surfaced using hand squeegees to provide complete and uniform coverage. If necessary, the area to be handworked shall be lightly dampened prior to mix placement. Handwork shall exhibit the same finish as that applied by the spreader box and shall be completed prior to final surfacing.

11.6 LINES

Care shall be taken to apply straight lines along curbs, shoulders, and intersections. No run-off on these areas will be permitted. Roofing felt or heavy plastic may be used to begin or end a pull cleanly. This also provides for easy removal of excess slurry.

11.7 ROLLING

Rolling is usually not necessary for slurry seal on roadways. Airports and parking areas should be rolled by a self-propelled, 10-ton (maximum) pneumatic tire roller equipped with a water spray system. All tires should be inflated per manufacturer's specifications. Rolling shall not start until the slurry has cured sufficiently to avoid damage by the roller. Areas which require rolling shall receive a minimum of two (2) full coverage passes.

11.8 CLEAN UP

All utility access areas, gutters and intersections, shall have the slurry seal removed as specified by the B.A.R. The contractor shall remove any debris associated with the performance of the work on a daily basis.

12. QUALITY CONTROL

12.1 INSPECTION

Inspectors assigned to projects must be familiar with the materials, equipment and application of slurry seal. Local conditions and specific project requirements should be considered when determining the parameters of field inspection.
Proper mix consistency should be one of the major areas of inspector concern. If mixes are

too dry, streaking, lumping and roughness will be present in the mat surface. Mixes applied too wet will flow excessively and not hold straight lane lines. Excessive liquids may also cause an asphalt-rich surface with segregation.

12.2 MATERIALS

To account for aggregate bulking, it is the responsibility of the contractor to check stockpile moisture content and to set the machine accordingly. At the B.A.R.'s discretion, material tests may be run on representative samples of the aggregate and emulsion. Tests will be run at the expense of the buyer. The buyer must notify the contractor immediately if any test fails to meet the specifications.

12.3 SLURRY SEAL

If required, representative samples of the slurry seal may be taken directly from the slurry unit(s). Consistency (ISSA TB No. 106) and residual asphalt content (ASTM D2172) tests may be run on the samples. Please note that the consistency test may not be applicable to certain Quick-Set and Quick-Traffic systems because of erratic results due to setting characteristics. If this test is run, it must be performed immediately after the sample is taken. Tests will be run at the expense of the buyer. The buyer must notify the contractor immediately if any test fails to meet specifications.

Data obtained from the proportioning devices on the slurry seal unit may be used to determine individual material quantities and application rate.

12.4 NON-COMPLIANCE

If any two successive tests fail on the stockpile aggregate, the job shall be stopped. If any two successive tests on the mix from the same machine fail, the use of the machine shall be suspended. It will be the responsibility of the contractor, at his expense, to prove to the B.A.R. that the problems have been corrected.

13. PAYMENT

The slurry seal shall be measured and paid for by the unit area or weight of aggregate and the weight of emulsion used on the work completed and accepted by the buyer. If paid by the weight of the aggregate and emulsified asphalt, the contractor shall submit to the B.A.R. certified delivery tickets which show quantities of each material delivered to the job site and used on the project. Payment shall be full compensation for all preparation, mixing and application of materials, and for all labor, equipment, tools, testing, cleaning, and incidentals necessary to complete the job as specified herein.

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ISSUED FOR
FOR REVIEW
DATE
4-17-18
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LAST EDIT DATE:
NOTE
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BAR SHOULD MEASURE 1" ON DRAWING IS NOT TO SCALE
SCALE:
DESIGNED:
DRAWN: JCS
CHECKED:
APPROVED:
CAD REF. FILE:

Bisnett Design Associates
Landscape Architecture & Environmental Planning
bda
16046 Bear Court
Grass Valley, CA 95949
530.277.3733
brian@bisnettdesign.com

DESERT HAVEN APARTMENTS
VICTORVILLE, CALIFORNIA
SLURRY SEAL SPECIFICATIONS

6-11-18

DRAWING: :
L-13

GROUND FLOOR CONVERSION NOTES

A, B, C - ORIGINAL MOTEL ROOM EFFICIENCY UNIT REFURBISH FINISHES & FIXTURES ONLY, SEE 5/A4.0.

M - MANAGER'S UNIT - REFURBISH FINISHES & FIXTURES ONLY

UNITS 1 AND 4 - COMBINE TWO ROOMS IN-LINE. SEE 1, 2/A4.0.

UNITS 2, 3, 5, 6, 7, 8, 9, 10 - COMBINE TWO ROOMS SIDE BY SIDE. SEE 3,4/A4.0.

COMMON ROOM - COMBINE FOUR ROOMS SIDE BY SIDE.

OFFICE, LAUNDRY - REFURBISH FINISHES ANY FIXTURES ONLY.

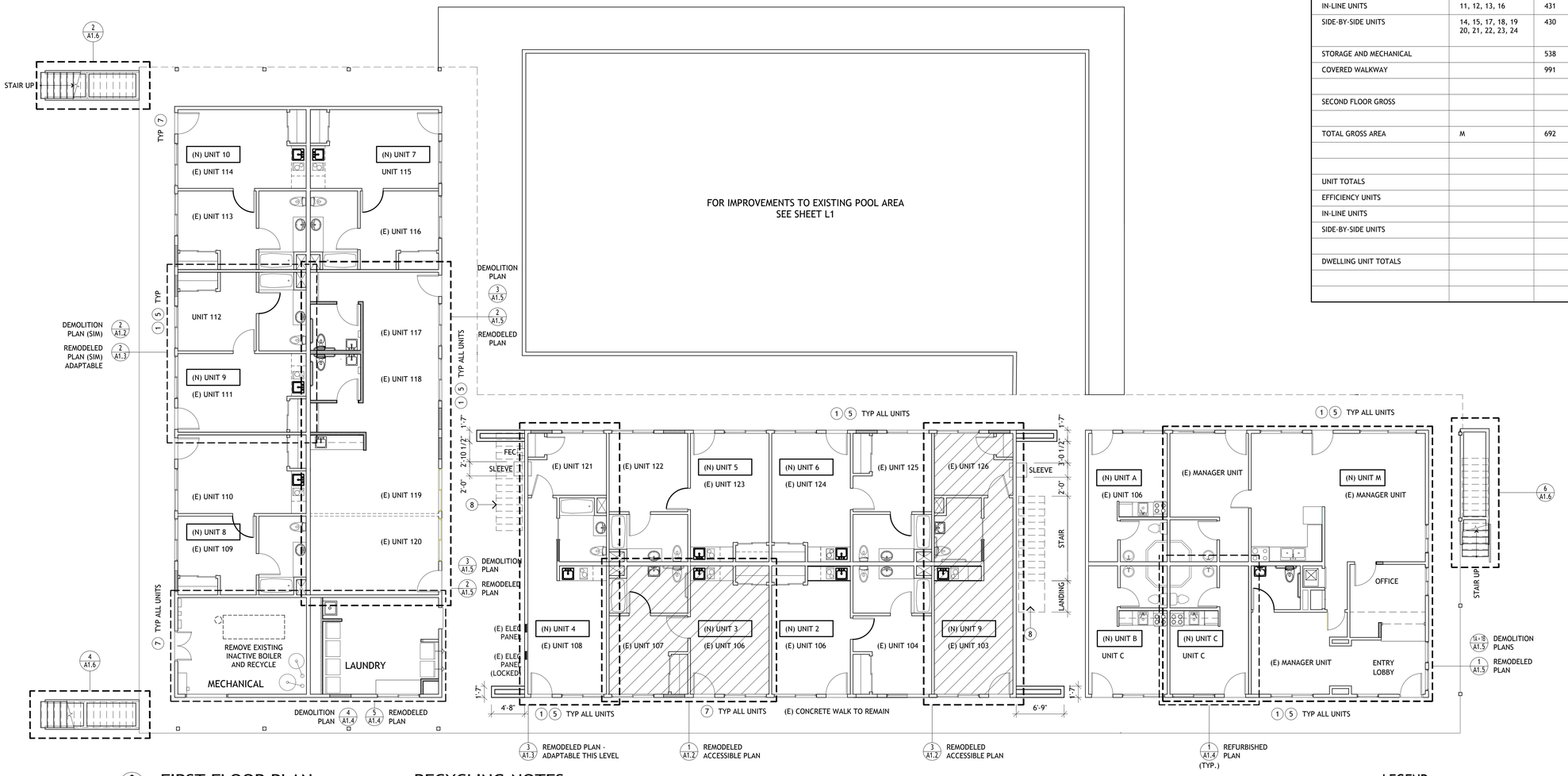
= ACCESSIBLE UNIT PER CBC 11B. ALL INLINE AND SIDE BY SIDE UNITS AT GROUND LEVEL TO BE ADAPTABLE PER CBC 11A EXCEPT 5% SHOWN AS ACCESSIBLE.

EXTERIOR KEY NOTES

1. INSTALL NEW UNIT ID WITH BRAILLE, TYPICAL AT ALL LEVEL UNITS.
2. NOT USED.
3. STAIRWAYS & RAILINGS: RETROFIT WITH NEW CLOSED RISERS, CBC RAILINGS AND EXTENSIONS.
4. WINDOWS AND DOORS: REPLACE ALL WINDOWS AND SLIDING GLASS DOORS, SOUND REDUCING, DUAL GLAZED INSULATED TYPE.
5. EXTERIOR DOORS: REPLACE ALL.
6. WATERPROOF ELEVATED WALKS.
7. SEAL ALL THRESHOLDS, REPLACE THRESHOLD WITH NEW CONFORMING THRESHOLD N.T.E $\frac{1}{2}$" AFF (CBC 1122A2.1).
8. REMOVE (E) STAIRS. INFILL RAILING ABOVE.

SUMMARY OF REMODELED UNITS

UNIT OR SPACES	UNIT NUMBERS	AREA (EACH) SF	QUANTITY	AREA SUB TOTAL SF
GROUND FLOOR				
MANAGER'S UNIT	M	692	1	692
OFFICE AREA		473	1	473
EFFICIENCY UNITS A, B AND C	A, B, C	221	3	663
IN-LINE UNITS 1 AND 4	1, 4	442	2	884
SIDE-BY-SIDE UNITS	2, 3, 5, 6, 7, 8, 9, 10	431	8	3, 448
COMMUNITY ROOM		868	1	868
BOILER ROOM		268	1	268
LAUNDRY ROOM		268	1	1, 021
COVERED WALKWAY		1021	1	1, 021
GROUND FLOOR GROSS AREA				8, 585
SECOND FLOOR				
EFFICIENCY UNITS	D, E, F, G	216	4	864
IN-LINE UNITS	11, 12, 13, 16	431	4	1 724
SIDE-BY-SIDE UNITS	14, 15, 17, 18, 19 20, 21, 22, 23, 24	430	10	4, 300
STORAGE AND MECHANICAL		538	1	538
COVERED WALKWAY		991	1	991
SECOND FLOOR GROSS				8, 417
TOTAL GROSS AREA	M	692	1	17, 002
UNIT TOTALS				
EFFICIENCY UNITS			7	
IN-LINE UNITS			6	
SIDE-BY-SIDE UNITS			18	
DWELLING UNIT TOTALS			31	



NORTH

FIRST FLOOR PLAN

SCALE: 1/8" = 1'-0"

LEGEND

(N) UNIT 10	REMODELED UNIT NUMBER WHEN CREATED FROM TWO EXISTING UNITS OR EXISTING UNIT RENUMBERED
(E) UNIT 114	EXISTING UNIT NUMBER FOR SPACE PROVIDED FOR ITS ASSOCIATION WITH LABELED UNIT NUMBERS ON ELECTRICAL PANELS OR TELEPHONE TERMINAL BLOCKS

RECYCLING NOTES

1 THE CONTRACTOR IS REQUIRED TO HAVE AND SUBMIT A WASTE MANAGEMENT PLAN TO THE CITY OF VICTORVILLE FOR REVIEW AND APPROVAL PRIOR TO START OF DEMOLITION AND CONSTRUCTION OPERATIONS.

2 THE PLAN AND THE DEMOLITION AND CONSTRUCTION RECYCLING SHALL COMPLY WITH REQUIREMENTS OF CALGREEN DIVISIONS 4.408, CONSTRUCTION WASTE REDUCTION, DISPOSAL AND RECYCLING, AND DIVISION 5.408.3.

AT A MINIMUM, 50% OF ALL DEMOLITION AND CONSTRUCTION WASTE TO BE RECYCLED.

3 WASTE TO BE RECYCLED OR SALVAGED TO INCLUDE, BUT NOT BE LIMITED TO THE FOLLOWING:

CARPET	SHINGLES	ALUMINUM
WOOD	WALLBOARD	GLASS
AGGREGATE	CARDBOARD (PACKAGING)	FERROUS METAL (RAILINGS)
PAINT		

4 THE CONTRACTOR IS REQUIRED TO HAVE AND SUBMIT A WASTE MANAGEMENT PLAN TO THE CITY OF VICTORVILLE FOR REVIEW AND APPROVAL PRIOR TO START OF DEMOLITION AND CONSTRUCTION OPERATIONS.

5 THE PLAN AND THE DEMOTION AND CONSTRUCTION RECYCLING SHALL COMPLY WITH REQUIREMENTS OF CALGREEN DIVISIONS 4.408, CONSTRUCTION WASTE REDUCTION, DISPOSAL AND RECYCLING, AND DIVISION 5.408.3.

6 TO VERIFY THE REQUIREMENTS OF CALIFORNIA AND CITY OF VICTORVILLE SEE:

VICTOR VALLEY RECYCLING GUIDE FOR CONSTRUCTION AND DEMOLITION MATERIALS

OR CONTACT THE CITY OF VICTORVILLE AT:

WWW.VICTORVILLECA.GOV
WWW.VICTORVILLRECYCLES.COM
720-955-8615

7 SALVAGED MATERIALS

RIGHT OF FIRST REFUSAL FOR SALVAGED MATERIALS TO BE OFFERED TO:
HABITAT FOR HUMANITY VICTORVILLE
16845 NORTH D STREET, VICTORVILLE, CA
760-241-3733

SALVAGEABLE MATERIALS TO INCLUDE, BUT NOT BE LIMITED TO:

UNIT ENTRY DOORS
DOOR HARDWARE
PLUMBING FIXTURES
LIGHTING FIXTURES

LEGEND

CROSS HATCHING INDICATES ACCESSIBLE UNIT

BASIS Architecture & Consulting

2130 FOURTH ST
SAN RAFAEL, CA 94901
PHONE (415) 457-6035
FAX (415) 457-6036

P.O.BOX 150539
SAN RAFAEL, CA 94915

CHARLES PICK, ARCHITECT

LICENSED ARCHITECT
CHARLES PICK
C-21406
6-2019
RENEWAL DATE
STATE OF CALIFORNIA

DRAWING REVISION LOG

FOR BIO	5-15-2018

PROJECT NAME:

DESERT HAVEN (QUEEN'S MOTEL) RE-DEVELOPMENT

PROJECT LOCATION:

16959 STODDARD WELLS RI
VICTORVILLE, CA 92395

SHEET TITLE:

REMODLELED GROUND FLOOR PLAN

SCALE: AS NOTED

A1.0

SECOND FLOOR CONVERSION NOTES:

D, E, F, G - ORIGINAL MOTEL ROOMS EFFICIENCY UNITS REFURBISH FIXTURES & FINISHES ONLY. SEE 5/A4.0.

UNITS 11, 12, 13, 16 - COMBINE TWO ROOMS IN-LINE. SEE 1, 2/A4.0.

UNITS 14, 15, 17, 18, 19, 20, 21, 22, 23, 24 - COMBINE TWO ROOMS SIDE BY SIDE. SEE 3,4/A4.0.

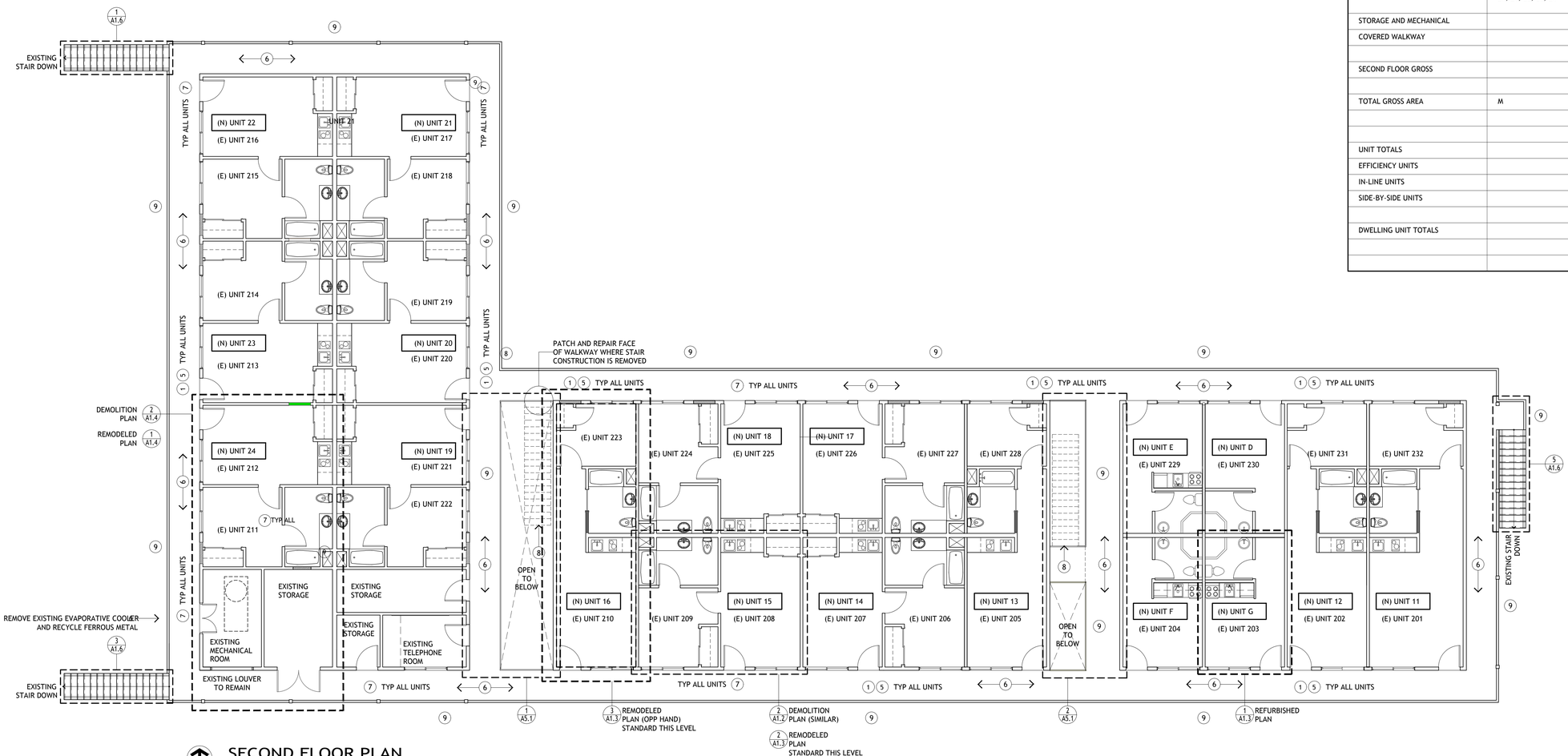
ALL UNITS THIS LEVEL ARE "STANDARD".

EXTERIOR KEY NOTES

1. INSTALL NEW UNIT ID WITH BRAILLE, TYPICAL AT ALL UNITS.
2. PROVIDE CANE RAIL DETECTION IN ACCORDANCE WITH CBC CHAPTER 11B- CALIFORNIA ACCESSIBILITY CODE, SEE SITE PLAN FOR LOCATIONS.
3. STAIRWAYS & RAILINGS: REPLACE ALL WITH NEW CLOSED RISERS, CBC RAILINGS AND EXTENSIONS.
4. WINDOWS AND DOORS: REPLACE ALL WINDOWS AND SLIDING GLASS DOORS, SOUND REDUCING, DUAL GLAZED INSULATED TYPE.
5. EXTERIOR DOORS: REPLACE ALL.
6. WATERPROOF ELEVATED WALKS. NOTE, SOME WALKS ARE SLOPED, PROVIDE NON SKID SURFACE.
7. SEAL ALL THRESHOLDS, REPLACE THRESHOLD WITH NEW CONFORMING THRESHOLD N.T.E $\frac{1}{2}$" AFF (CBC 1122A2.1).@ GROUND FLOOR.
8. REMOVE (E) STAIRS.
9. (N) GUARDRAIL - CONTINUOUS

SUMMARY OF REMODELED UNITS

UNIT OR SPACES	UNIT NUMBERS	AREA (EACH) SF	QUANTITY	AREA SUB TOTAL SF
GROUND FLOOR				
MANAGER'S UNIT	M	692	1	692
OFFICE AREA		473	1	473
EFFICIENCY UNITS A, B AND C	A, B, C	221	3	663
IN-LINE UNITS 1 AND 4	1, 4	442	2	884
SIDE-BY-SIDE UNITS	2, 3, 5, 6, 7, 8, 9, 10	431	8	3, 448
COMMUNITY ROOM		868	1	868
BOILER ROOM		268	1	268
LAUNDRY ROOM		268	1	1, 021
COVERED WALKWAY		1021	1	1, 021
GROUND FLOOR GROSS AREA				8, 585
SECOND FLOOR				
EFFICIENCY UNITS	D, E, F, G	216	4	864
IN-LINE UNITS	11, 12, 13, 16	431	4	1 724
SIDE-BY-SIDE UNITS	14, 15, 17, 18, 19 20, 21, 22, 23, 24	430	10	4, 300
STORAGE AND MECHANICAL		538	1	538
COVERED WALKWAY		991	1	991
SECOND FLOOR GROSS				8, 417
TOTAL GROSS AREA	M	692	1	17, 002
UNIT TOTALS				
EFFICIENCY UNITS			7	
IN-LINE UNITS			6	
SIDE-BY-SIDE UNITS			18	
DWELLING UNIT TOTALS			31	



SECOND FLOOR PLAN

SCALE: 1/8" = 1'-0"

NORTH

LEGEND

(N) UNIT 10	REMODELED UNIT NUMBER WHEN CREATED FROM TWO EXISTING UNITS OR EXISTING UNIT RENUMBERED
(E) UNIT 114	EXISTING UNIT NUMBER FOR SPACE PROVIDED FOR ITS ASSOCIATION WITH LABELED UNIT NUMBERS ON ELECTRICAL PANELS OR TELEPHONE TERMINAL BLOCKS

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DESERT HAVEN
(QUEEN'S MOTEL)
RE-DEVELOPMENT

PROJECT LOCATION:

16959 STODDARD WELLS RI
VICTORVILLE, CA 92395

SHEET TITLE:

REMODELED
SECOND FLOOR PLAN

A1.1

UNIT KEYED NOTES

KITCHENETTE

1 COUNTERTOPS: INSTALL NEW SOLID SURFACE COUNTERTOP AT 33 3/4" AFF W/4" BACKSPLASH.

2 INSTALL NEW ACME 72" ADA CERTIFIED MODULAR KITCHENETTE WITH DOUBLE BURNER HOT PLATE, REFRIGERATOR AND INTEGRATED SINK. (NO UPPER CABINETS). ALLOWS FOR ACCESSIBILITY AND UTILITY.

BATHROOM

3 (N) DROPPED CLG @ 7'-0", MOISTURE RESISTANT DRYWALL, 2x6 @ 16" W/LEDGERS @ BEARING ENDS. TAPE, SEAL, TEXTURE TO MATCH (E) (AT IN-LINE UNITS)

4 INSTALL NEW WATER SENSE TOILETS, SHOWER HEADS & SHOWER VALVES WITH ANTI-SCALD.

5 INSTALL CONFORMING LOW FLOW TOILET W/FLUSH LEVER AT WIDE SIDE W/REQ'D CLEARANCES. PROVIDE CONFORMING FRONT & SIDE GRAB BARS WITH REQ'D. BLOCKING.

6 INSTALL CONFORMING WALL HANG LAVATORY W/LEVER LOW FLOW FAUCET & PIPE WRAP, SINK RIM HEIGHT NTE 34" AFF.

INTERIOR

7 PROVIDE CONFORMING DOOR LEVER HARDWARE WITH RETURNS AT ALL SWING DOORS. ALL DOOR HARDWARE WITH KEY LOCKING SHALL BE SINGLE HAND OPERABLE, PER DISABLED ACCESSIBLE STANDARDS.

8 THRESHOLD: REPLACE THRESHOLD WITH NEW CONFORMING THRESHOLD N.T.E. 1/2 INCH A.F.F. [CBC 1132A.1, 1126A.2.1]

9 RETROFIT WINDOWS. FRAME AND WATERPROOF PER STANDARD WINDOW INSTALLATION. DUAL GLAZED, LOW-E GLASS, ENERGY STAR-RATED. OPERATING HARDWARE SHALL BE COMPLIANT FOR FRONT & SIDE REACH.

10 PROVIDE DISABLED ACCESS DOOR PULL HARDWARE AT CLOSETS AND POCKET DOORS.

11 LOWER SHELF & POLE @ 47" AFF MAX. AT CLOSETS.

MECHANICAL/PLUMBING/ELECTRICAL

12 INSTALL HARDWIRED AUDIBLE/ VISUAL SMOKE DETECTORS COMBINED WITH CARBON MONOXIDE DETECTOR WITH FLASHING STROBE (MOUNT AT 80" AFF TO BOTTOM SURFACE MINIMUM). PROVIDE AT COMMON USE LAUNDRY ROOMS AND DISABLED ACCESSIBLE UNITS.

13 PROVIDE HARD-WIRED ELECTRIC W/ FLASHING STROBE DOORBELL OUTSIDE THE DWELLING UNIT PRIMARY ENTRANCE. MOUNT BOTTOM SURFACE AT 80" AFF. ACTIVATION OF THE BUTTON OR SWITCH IS TO INITIATE AN AUDIBLE TONE AND VISIBLE SIGNAL WITHIN THE RESIDENTIAL DWELLING UNIT. WHEN VISIBLE DOORBELL SIGNALS ARE LOCATED IN SLEEPING AREAS, THEY MUST HAVE CONTROLS TO DEACTIVATE THE SIGNAL. [CBC 11B809.5.5.1]

14 REPLACE LIGHT FIXTURES W/ HIGH EFFICIENCY, HIGH EFFICACY LIGHTING (SEE ELECTRICAL DRAWINGS).

15 OUTLETS AT KITCHEN SPACE TO PROVIDE NO MORE THAN 24" TO OUTLET AT ALL COUNTER AREAS. (SEE ELECTRICAL DRAWINGS). PROVIDE 1 USB OUTLET IN KITCHEN.

16 RE-PIPE ALL COPPER SUPPLY LINES IN UNITS (SEE PLUMBING DRAWINGS).

17 UNIT HVAC: REUSE EXISTING SYSTEM. REFURBISH AS NEEDED, TYPICAL.

18 PROVIDE/REPLACE ALL ANGLE STOPS.

19 PLUMBING FIXTURES - KITCHENETTE: PROVIDE WATER AND DRAIN FOR NEW KITCHENETTE, TYPICAL.

20 BATH FAN VENTILATION: REPLACE ALL BATHROOM FANS WITH HUMIDISTAT CONTROLLED, CONSTANT RUNNING TWO SPEED ESTAR UNITS. INCLUDES RADIANT DAMPER FOR ONE HOUR FLOOR/CEILING LOCATIONS, TYPICAL.

21 KITCHENETTE VENTILATION: ADD THROUGH WALL VENT FAN, WATERPROOF FULLY, TYPICAL.

ROOM FINISH SCHEDULE
TYPICAL SIDE-BY-SIDE ACCESSIBLE GROUND FLOOR UNIT

ROOM #	ROOM DESCRIPTION	FLOOR	BASE	WALLS	CEILING	NOTES
100	ACCESSIBLE REMODELED LIVING ROOM	NEW VINYL ON EXISTING CONCRETE SLAB ON GRADE	NEW 4" COVE VINYL BASE	EXISTING AND NEW GYP BRD TO BE PAINTED	EXISTING GYP BRD TO BE PAINTED	- - - -
101	ACCESSIBLE REMODELED BEDROOM	NEW VINYL ON EXISTING CONCRETE SLAB ON GRADE	NEW 4" COVE VINYL BASE	EXISTING GYP BRD TO BE PAINTED	EXISTING GYP BRD TO BE PAINTED	- - - -
102	ACCESSIBLE REMODELED BATHROOM	NEW VINYL ON EXISTING CONCRETE SLAB ON GRADE	NEW 4" COVE VINYL BASE	EXISTING GYP BRD TO BE PAINTED	(N) OR (E) GYP BRD TO BE PAINTED	- - - -

GENERAL NOTES FOR INTERIOR FINISHES:

1 NEW GYP BRD REQUIRED IN ANY CONSTRUCTION TO HAVE TEXTURED FINISH TO MATCH EXISTING ADJACENT FINISH

2 ALL NEW PAINT COLORS TO BE SELECTED BY OWNER FROM STANDARD COLORS

3 CONSTRUCTION CLEANING REQUIRED TO INCLUDE, BUT NOT BE LIMITED TO, TO THOROUGHLY CLEANING ANY EXISTING FIXTURES, FAUCETS, LIGHTING FIXTURES, ETC., TO BE REUSED IN NEW WORK.

GENERAL NOTES

1 DRYWALL REPAIRS AT UNITS AS NEEDED.

2 WINDOW WATERPROOFING: REMOVE SEALANTS ALL FOUR SIDES INCLUDING COMPLETE REMOVAL AT BOTTOM CORNERS FOR WEEPS. ADD NEW POLYURETHANE SEALANTS TOP AND SIDES. COORDINATE WITH OTHER WATERPROOFING.

3 NEW INTERIOR PARTITION WALLS. FINISH, PRIME & PAINT, AS NEEDED.

4 UNIT PAINT AND DRYWALL: MINOR REPAIRS, AND PAINT ALL SURFACES INCLUDING DOORS.

5 PROVIDE NEW RESILIENT FLOORING THROUGHOUT, TYP. AT GROUND LEVEL UNITS.

6 PROVIDE CARPET FLOORING THROUGHOUT, TYP. AT UPPER LEVEL UNITS.

7 PROVIDE NEW RESILIENT FLOORING AT BATHROOMS, TYP. AT UPPER LEVEL UNITS.

MISCELLANEOUS LINTEL SCHEDULE

1 NEW 3'-0" OPENINGS IN BEARING AND NON-BEARING WALLS TO HAVE NEW 4" X 6" LINTELS BEARING ON A MINIMUM OF TWO (2) STUDS EACH END OR TWO (2) 2" X6" WITH 1/2" PLYWOOD FLITCH PLATE

ROOM FINISH SCHEDULE
TYPICALIN-LINE ACCESSIBLE GROUND FLOOR ACCESSIBLE UNIT

ROOM #	ROOM DESCRIPTION	FLOOR	BASE	WALLS	CEILING	NOTES
100	ACCESSIBLE REMODELED LIVING ROOM	NEW VINYL ON EXISTING CONCRETE SLAB ON GRADE	NEW 4" COVE VINYL BASE	EXISTING AND NEW GYP BRD TO BE PAINTED	EXISTING GYP BRD TO BE PAINTED	- - - -
101	ACCESSIBLE REMODELED BEDROOM	NEW VINYL ON EXISTING CONCRETE SLAB ON GRADE	NEW 4" COVE VINYL BASE	EXISTING GYP BRD TO BE PAINTED	EXISTING GYP BRD TO BE PAINTED	- - - -
102	ACCESSIBLE REMODELED BATHROOM	NEW VINYL ON EXISTING CONCRETE SLAB ON GRADE	NEW 4" COVE VINYL BASE	EXISTING GYP BRD TO BE PAINTED	EXISTING GYP BRD TO BE PAINTED	- - - -

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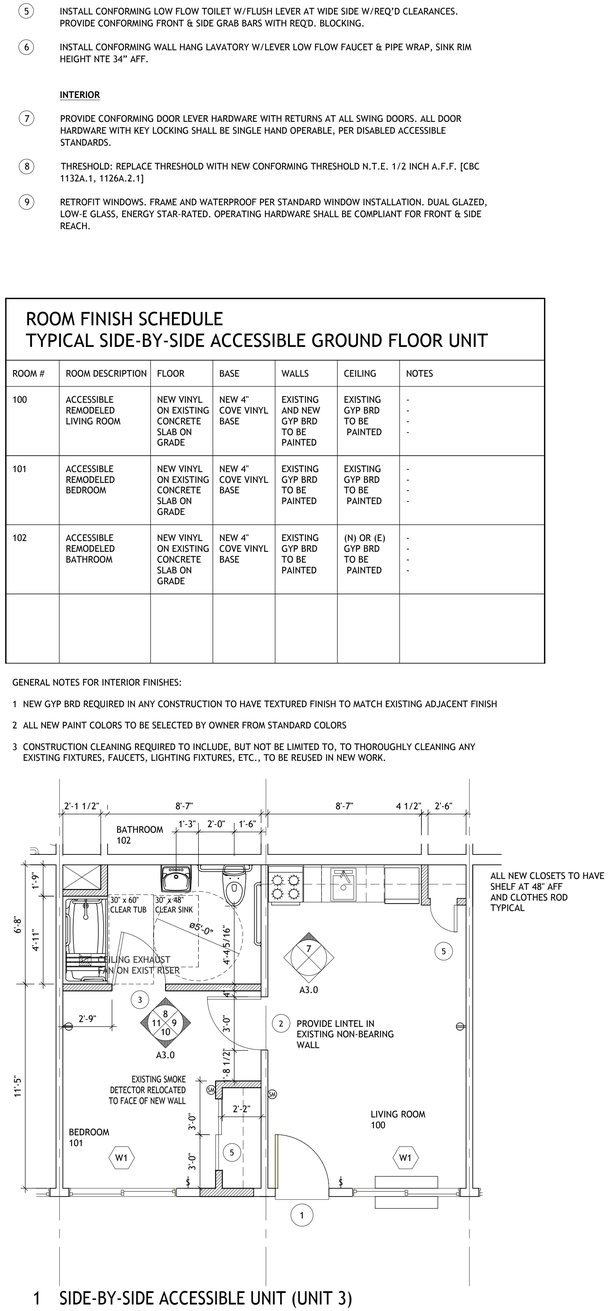
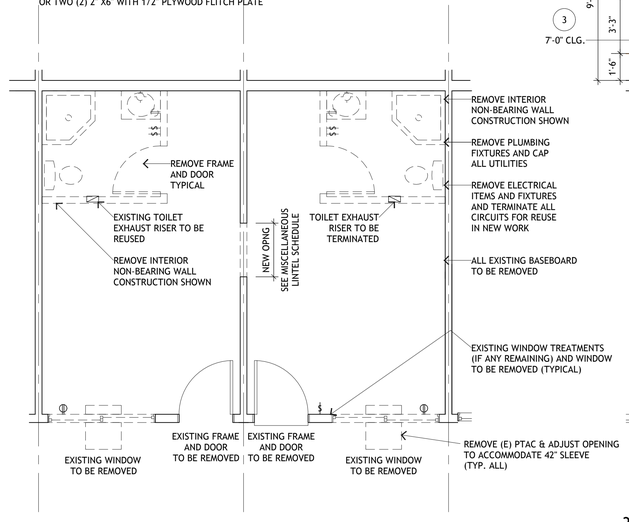
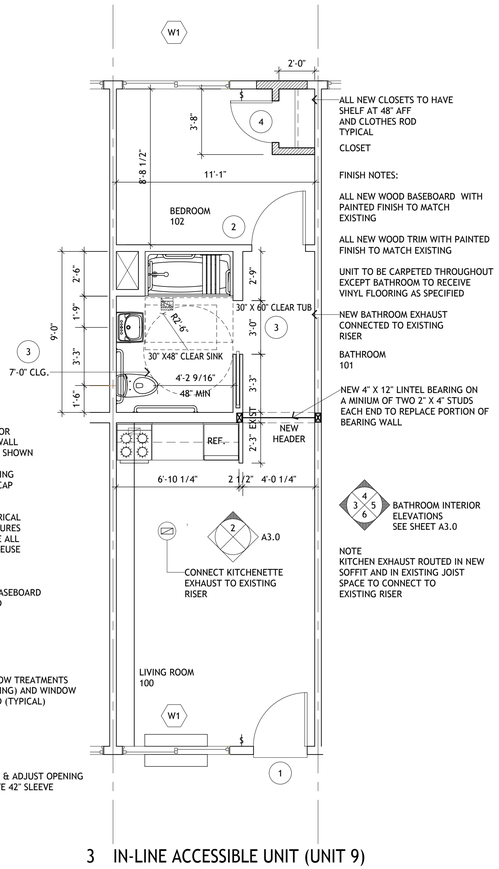
4 TAPE, SEAL AND TEXTURE TO MATCH (E) AT EXPANDED PTAC SLEEVES. TRIM W/ (N) WOOD.



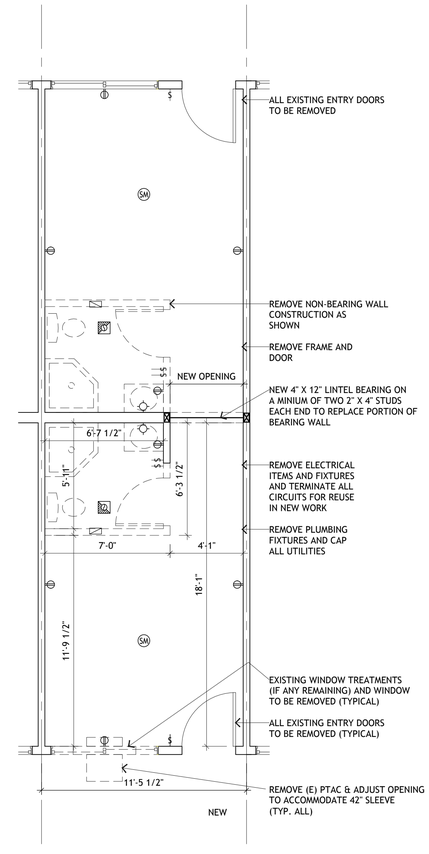
1 SIDE-BY-SIDE ACCESSIBLE UNIT (UNIT 3)
REMODELED PLAN
SCALE: 1/4" = 1'-0"



2 TYPICAL SIDE BY SIDE UNIT DEMOLITION PLAN
SCALE: 1/4" = 1'-0"



3 IN-LINE ACCESSIBLE UNIT (UNIT 9)
REMODELED PLAN
SCALE: 1/4" = 1'-0"



4 TYPICAL IN-LINE UNIT DEMOLITION PLAN
SCALE: 1/4" = 1'-0"

BASIS Architecture & Consulting

2130 FOURTH ST
SAN RAFAEL, CA 94901
PHONE (415) 457-6035
FAX (415) 457-6036

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FOR BIO	5-15-2018

PROJECT NAME:

DESERT HAVEN
(QUEEN'S MOTEL)
RE-DEVELOPMENT

PROJECT LOCATION:

16959 STODDARD WELLS RI
VICTORVILLE, CA 92395

SHEET TITLE:

TYPICAL
UNIT PLANS

A1.2

MISCELLANEOUS ELECTRICAL NOTES (ALSO, SEE ELECTRICAL PLANS)

1 IN ANY NEWLY CREATED SLEEPING ROOM, THE EXISTING AND NEW RECEPTACLES SHALL BE BE CONNECTED TO AN NEW ARC FAULT BREAKER IN THE ELECTRICAL PANEL THAT REPLACES THE EXISTING.
SEE ELECTRICAL DRAWINGS.

2 IN EXISTING MANAGER'S UNIT (NO CHANGES) THE EXISTING SMOKE AND CARBON MONOXIDE ALARMS ARE BATTERY OPERATED AND SHALL BE EXISTING TO REMAIN.

3 IN THE NEW (REMODELED) COMMUNITY ROOM AND REMODELED DWELLING UNITS, THE SMOKE AND CARBON MONOXIDE ALARMS SHALL BE POWERED FROM THE BUILDINGS ELECTRICAL POWER SYSTEM AND SHALL HAVE INTEGRAL BATTERY BACK UP.

4 NEW COMBINATION SMOKE AND CARBON MONOXIDE ALARMS SHALL BE LOCATED WITHIN PROXIMITY TO THE NEW GAS FUELED RANGES.

5 A COMBINATION SMOKE AND CARBON MONOXIDE ALARM IN EACH UNIT SHALL BE INTERCONNECTED IN SUCH A MANNER THAT ACTUATION OF ONE SHALL CAUSE ALL OTHER ALARMS IN THE UNIT TO BE ACTIVATED (CBC 907.2.11, CRC R314, CRC R315).

6 A SMOKE ALARM, POWERED BY THE BUILDINGS FROM THE PERMANENT BUILDINGS ELECTRICAL SYSTEM SHALL BE PROVIDED IN EVERY SLEEPING ROOM AND SHALL HAVE INTEGRAL BATTERY BACKUP.

7 ALL SMOKE AND CARBON MONOXIDE ALARMS SHALL BE ON A CIRCUIT PROTECTED WITH A COMBINATION ARC FAULT CIRCUIT INTERRUPTER (2010 CEC 210.12).

8 IN GROUND FLOOR ACCESSIBLE DWELLING UNITS ALARM SHALL HAVE AUDIBLE AND VISUAL ALARMS.

9 AUDIBLE ALARMS SHALL PRODUCE A SOUND THAT EXCEEDS THE PREVAILING EQUIVALENT SOUND LEVEL IN THE ROOM OR SPACE BY AT LEAST 15 DECIBELS OR EXCEEDS ANY MAXIMUM SOUND LEVEL WITH A DURATION OF 30 SECONDS BY 5 DECIBELS, WHICHEVER IS LOUDER.
SOUND LEVELS FOR ALARM SIGNALS SHALL NOT EXCEED 120 DECIBELS.
(UFAS 4.28.2)

THE FLASHING FREQUENCY OF VISUAL ALARM DEVICES SHALL BE LESS THAN 5 HZ. IF SUCH ALARMS USE ELECTRICITY FROM THE BUILDING AS A POWER SOURCE, THEN THEY SHALL BE INSTALLED ON THE SAME SYSTEM AS THE AUDIBLE EMERGENCY ALARMS.

10 GROUND FAULT CIRCUIT INTERRUPTION PROTECTION (GFCI)

GROUND-FAULT CIRCUIT-INTERRUPTION (GFCI) BE PROVIDED AS REQUIRED IN SECTION 210.8 (A)

IN DWELLING UNITS, ALL 12-VOLT, SINGLE PHASE, 15 AND 20 AMPERE RECEPTACLES INSTALLED IN THE LOCATIONS SPECIFIED IN 210.8(A)(1) THROUGH (10) SHALL HAVE GFCI PROTECTION.

IN NEWLY REMODELED BATHROOMS - ALL RECEPTACLES ARE REQUIRED TO BE GFCI PROTECTED. INCLUDING RECEPTACLES WITHIN 6 FEET OF THE OUTSIDE EDGE OF A BATHTUB OR SHOWER STALL

IN NEWLY REMODELED KITCHENS - WHERE THE RECEPTACLES ARE INSTALLED TO SERVE THE COUNTERTOP SURFACES AND RECEPTACLES WITHIN 6 FEET OF A WET SINK ARE REQUIRED TO BE GFCI PROTECTED.

IN NEWLY REMODELED COMMON LAUNDRY ALL RECEPTACLES INSTALLED SHALL HAVE GFCI PROTECTION.

ROOM FINISH SCHEDULE
TYPICAL NON-ACCESSIBLE EFFICIENCY UNIT (GROUND OR SECOND)

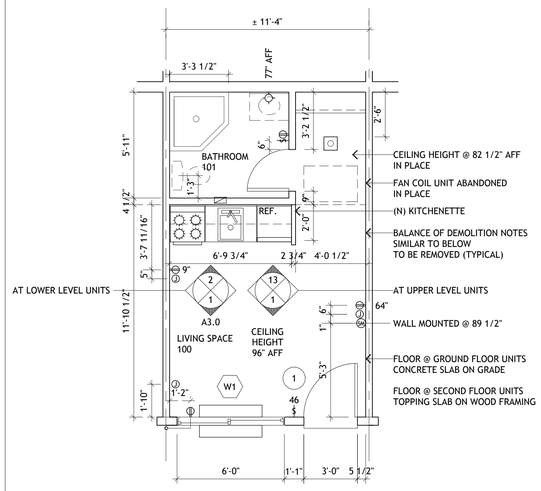
ROOM #	ROOM DESCRIPTION	FLOOR	BASE	WALLS	CEILING	NOTES
100	LIVING ROOM	NEW VINYL ON EXISTING CONCRETE SLAB ON GRADE (GROUND FLOOR) OR TOPPING SLAB (SECOND FLOOR)	NEW 4" COVE VINYL BASE	EXISTING GYP BRD TO BE PAINTED	EXISTING GYP BRD TO BE PAINTED	PATCH AND REPAIR EXISTING GYP BRD SURFACES AS REQD - -
101	BATHROOM	NEW VINYL ON EXISTING CONCRETE SLAB ON GRADE (GROUND FLOOR) OR TOPPING SLAB (SECOND FLOOR)	NEW 4" COVE VINYL BASE	EXISTING GYP BRD TO BE PAINTED	(N) OR (E) GYP BRD TO BE PAINTED	PATCH AND REPAIR EXISTING GYP BRD SURFACES AS REQD - -

GENERAL NOTES FOR INTERIOR FINISHES:

1 NEW GYP BRD REQUIRED IN ANY CONSTRUCTION TO HAVE TEXTURED FINISH TO MATCH EXISTING ADJACENT FINISH

2 ALL NEW PAINT COLORS TO BE SELECTED BY OWNER FROM STANDARD COLORS

3 CONSTRUCTION CLEANING REQUIRED TO INCLUDE, BUT NOT BE LIMITED TO, TO THOROUGHLY CLEANING ANY EXISTING FIXTURES, FAUCETS, LIGHTING FIXTURES, ETC., TO BE REUSED IN NEW WORK.



1 TYPICAL EFFICIENCY UNIT
SCALE: 1/4" = 1'-0"

ROOM FINISH SCHEDULE
TYPICAL SIDE-BY-SIDE NON-ACCESSIBLE SECOND FLOOR UNIT

ROOM #	ROOM DESCRIPTION	FLOOR	BASE	WALLS	CEILING	NOTES
100	ACCESSIBLE REMODELED LIVING ROOM	NEW VINYL ON EXISTING CONCRETE SLAB ON GRADE	NEW 4" COVE VINYL BASE	EXISTING AND NEW GYP BRD TO BE PAINTED	EXISTING GYP BRD TO BE PAINTED	- - - -
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102	ACCESSIBLE REMODELED BATHROOM	NEW VINYL ON EXISTING CONCRETE SLAB ON GRADE	NEW 4" COVE VINYL BASE	EXISTING GYP BRD TO BE PAINTED	(N) OR (E) GYP BRD TO BE PAINTED	- - - -

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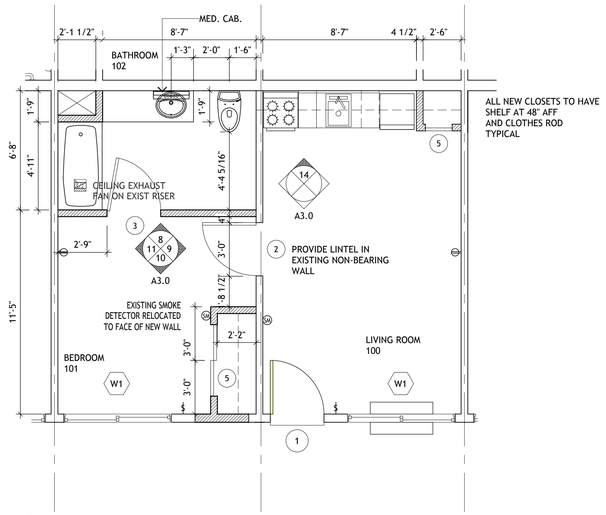

2 TYPICAL SIDE-BY-SIDE STANDARD UNIT (ADAPTABLE AT FIRST FLOOR) REMODELED PLAN
SCALE: 1/4" = 1'-0"

ROOM FINISH SCHEDULE
TYPICAL IN-LINE NON-ACCESSIBLE SECOND FLOOR UNIT

ROOM #	ROOM DESCRIPTION	FLOOR	BASE	WALLS	CEILING	NOTES
100	ACCESSIBLE REMODELED LIVING ROOM	NEW VINYL ON EXISTING CONCRETE SLAB ON GRADE	NEW 4" COVE VINYL BASE	EXISTING AND NEW GYP BRD TO BE PAINTED	EXISTING GYP BRD TO BE PAINTED	- - - -
101	ACCESSIBLE REMODELED BEDROOM	NEW VINYL ON EXISTING CONCRETE SLAB ON GRADE	NEW 4" COVE VINYL BASE	EXISTING GYP BRD TO BE PAINTED	EXISTING GYP BRD TO BE PAINTED	- - - -
102	ACCESSIBLE REMODELED BATHROOM	NEW VINYL ON EXISTING CONCRETE SLAB ON GRADE	NEW 4" COVE VINYL BASE	EXISTING GYP BRD TO BE PAINTED	(N) OR (E) GYP BRD TO BE PAINTED	- - - -

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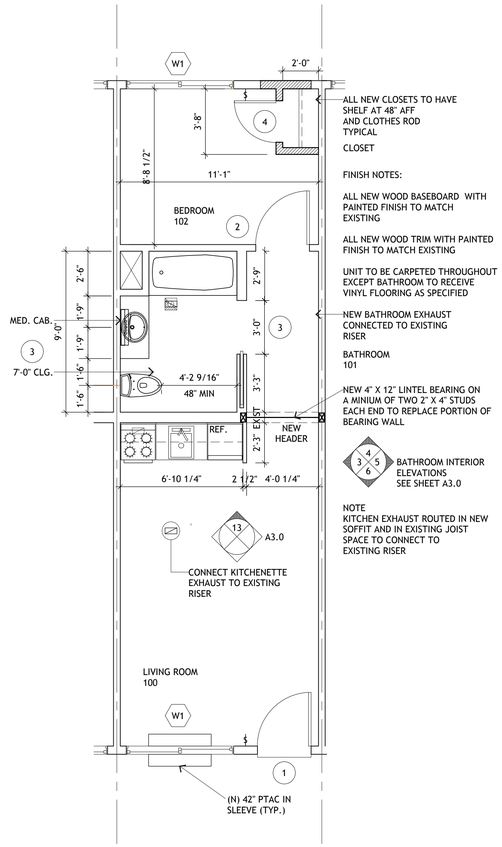
4 TAPE AND SEAL AND TEXTURE TO MATCH (E) AT EXPANDED PTAC SLEEVES. TRIM W/ (N) WOOD.

5 SEE INTERIOR ELEVATIONS FOR ADAPTABILITY REQUIREMENTS.



3 TYPICAL IN-LINE STANDARD UNIT (ADAPTABLE AT FIRST FLOOR) REMODELED PLAN
SCALE: 1/4" = 1'-0"

BASIS Architecture & Consulting

2130 FOURTH ST
SAN RAFAEL, CA 94901
PHONE (415) 457-6035
FAX (415) 457-6036

P.O.BOX 150539
SAN RAFAEL, CA 94915

CHARLES PICK, ARCHITECT

LICENSED ARCHITECT
CHARLES PICK
C-21406
6-2019
RENEWAL DATE
STATE OF CALIFORNIA

DRAWING REVISION LOG

FOR BIO	5-15-2018

PROJECT NAME:

DESERT HAVEN
(QUEEN'S MOTEL)
RE-DEVELOPMENT

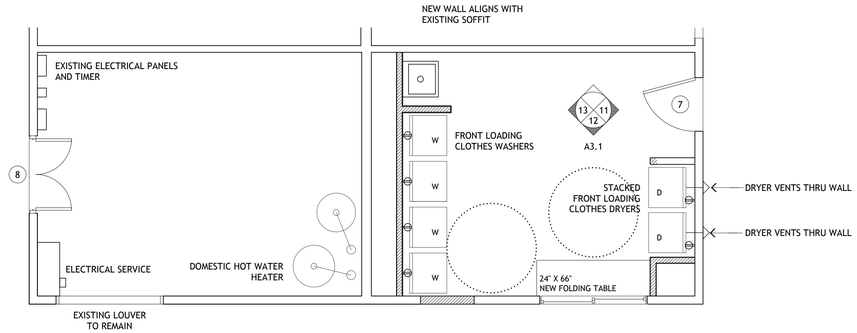
PROJECT LOCATION:

16959 STODDARD WELLS RD
VICTORVILLE, CA 92395

SHEET TITLE:

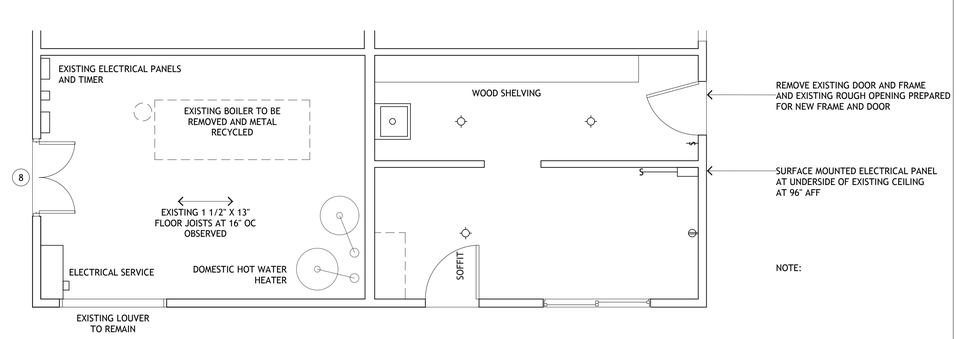
TYPICAL UNIT
ADAPTABLE PLANS

A1.3



4 REMODELED COMMON LAUNDRY PLAN

SCALE: 1/4" = 1'-0"



5 EXISTING CONDITIONS AND DEMOLITION - LAUNDRY AND MECHANICAL ROOM

SCALE: 1/4" = 1'-0"

ROOM FINISH SCHEDULE
UNITS 211 + 212

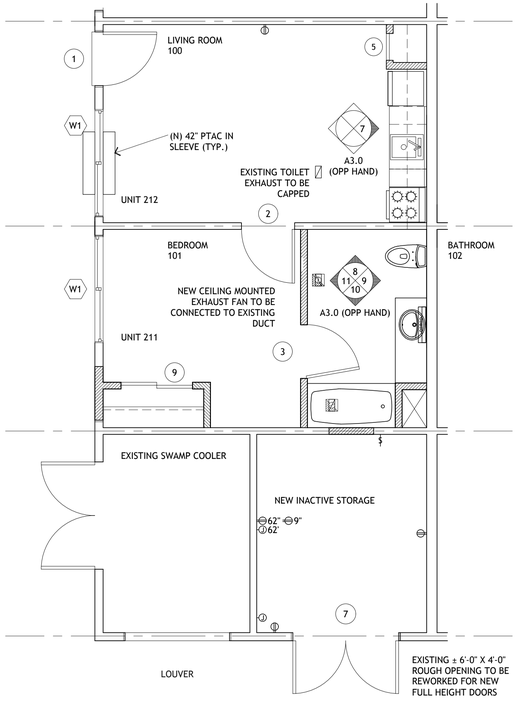
ROOM #	ROOM DESCRIPTION	FLOOR	BASE	WALLS	CEILING	NOTES
100	REMODELED LIVING ROOM	NEW VINYL ON EXISTING CONCRETE TOPPING SLAB	NEW 4" COVE VINYL BASE	EXISTING AND NEW GYP BRD TO BE PAINTED	EXISTING GYP BRD TO BE PAINTED	- - - -
101	REMODELED BEDROOM	NEW VINYL ON EXISTING CONCRETE TOPPING SLAB	NEW 4" COVE VINYL BASE	EXISTING GYP BRD TO BE PAINTED	EXISTING GYP BRD TO BE PAINTED	- - - -
102	REMODELED BATHROOM	NEW VINYL ON EXISTING CONCRETE TOPPING SLAB	NEW 4" COVE VINYL BASE	EXISTING GYP BRD TO BE PAINTED	EXISTING GYP BRD TO BE PAINTED	- - - -

GENERAL NOTES FOR INTERIOR FINISHES:

1 NEW GYP BRD REQUIRED IN ANY CONSTRUCTION TO HAVE TEXTURED FINISH TO MATCH EXISTING ADJACENT FINISH

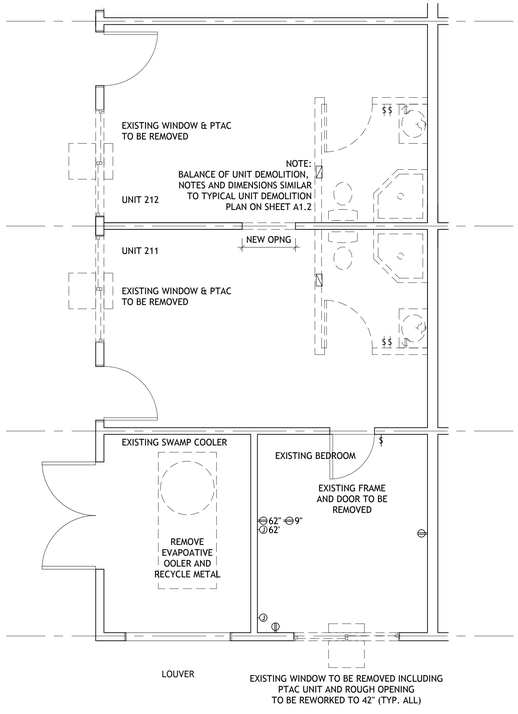
2 ALL NEW PAINT COLORS TO BE SELECTED BY OWNER FROM STANDARD COLORS

3 CONSTRUCTION CLEANING REQUIRED TO INCLUDE, BUT NOT BE LIMITED TO, TO THOROUGHLY CLEANING ANY EXISTING FIXTURES, FAUCETS, LIGHTING FIXTURES, ETC., TO BE REUSED IN NEW WORK.



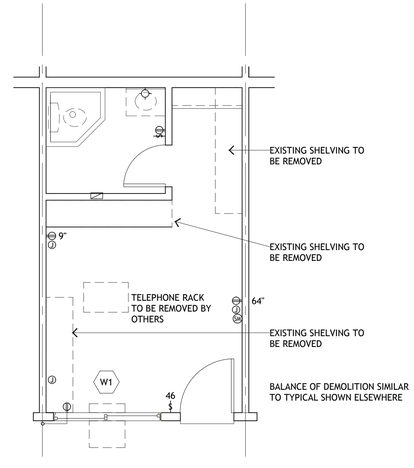
1 REMODELED PLAN UNITS 211 + 212

SCALE: 1/4" = 1'-0"



2 DEMOLITION PLAN UNITS 211 + 212

SCALE: 1/4" = 1'-0"



3 ADDITIONAL DEMOLITION REQD IN UNIT 107

SCALE: 1/4" = 1'-0"

BASIS Architecture & Consulting

2130 FOURTH ST
SAN RAFAEL, CA 94901
PHONE (415) 457-6035
FAX (415) 457-6036

P.O.BOX 150539
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(QUEEN'S MOTEL)
RE-DEVELOPMENT

PROJECT LOCATION:

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VICTORVILLE, CA 92395

SHEET TITLE:

DETAIL
PLANS

A1.4

ROOM FINISH SCHEDULE
GROUND FLOOR MANAGER'S UNIT

ROOM #	ROOM DESCRIPTION	FLOOR	BASE	WALLS	CEILING	NOTES
M - 100	LIVING ROOM	NEW VINYL ON EXISTING CONCRETE SLAB ON GRADE	NEW 4" COVE VINYL BASE	EXISTING GYP BRD TO BE PAINTED	EXISTING GYP BRD TO BE PAINTED	- - - -
M - 101	DINING ROOM	NEW VINYL ON EXISTING CONCRETE SLAB ON GRADE	NEW 4" COVE VINYL BASE	EXISTING GYP BRD TO BE PAINTED	EXISTING GYP BRD TO BE PAINTED	- - - -
M - 102	KITCHEN	NEW VINYL ON EXISTING CONCRETE SLAB ON GRADE	NEW 4" COVE VINYL BASE	EXISTING GYP BRD TO BE PAINTED	EXISTING GYP BRD TO BE PAINTED	- - - -
M - 103	BEDROOM	NEW VINYL ON EXISTING CONCRETE SLAB ON GRADE	NEW 4" COVE VINYL BASE	EXISTING GYP BRD TO BE PAINTED	EXISTING GYP BRD TO BE PAINTED	- - - -
M - 104	BATHROOM	NEW VINYL ON EXISTING CONCRETE SLAB ON GRADE	NEW 4" COVE VINYL BASE	EXISTING GYP BRD TO BE PAINTED	EXISTING GYP BRD TO BE PAINTED	- - - -

ROOM FINISH SCHEDULE
ACCESSIBLE GROUND FLOOR OFFICE

ROOM #	ROOM DESCRIPTION	FLOOR	BASE	WALLS	CEILING	NOTES
O - 100	EXISTING LOBBY	NEW VINYL ON EXISTING CONCRETE SLAB ON GRADE	NEW 4" COVE VINYL BASE	EXISTING AND NEW GYP BRD TO BE PAINTED	EXISTING GYP BRD TO BE PAINTED	SEE SHEET A5.0 FOR NEW ACCESSIBLE SERVICE COUNTER -
O - 101	EXISTING PUBLIC SPACE BEDROOM	NEW VINYL ON EXISTING CONCRETE SLAB ON GRADE	NEW 4" COVE VINYL BASE	EXISTING GYP BRD TO BE PAINTED	EXISTING GYP BRD TO BE PAINTED	- - - -
O - 102	NEW UNISEX ACCESSIBLE TOILET	NEW VINYL ON EXISTING CONCRETE SLAB ON GRADE	NEW 4" COVE VINYL BASE	EXISTING GYP BRD TO BE PAINTED	EXISTING GYP BRD TO BE PAINTED	- - - -

ROOM FINISH SCHEDULE
ACCESSIBLE GROUND FLOOR COMMUNITY ROOM

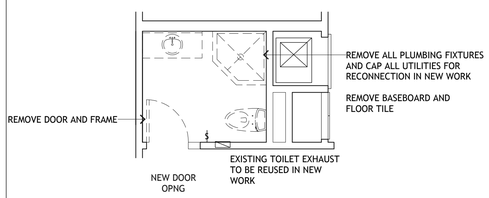
ROOM #	ROOM DESCRIPTION	FLOOR	BASE	WALLS	CEILING	NOTES
100	MULTI-PURPOSE ROOM LIVING ROOM	NEW VINYL ON EXISTING CONCRETE SLAB ON GRADE	NEW 4" COVE VINYL BASE	GYP BRD TO BE PAINTED	~~GYP BRD~~ PAINTED	- - - -
101	WOMEN'S ACCESSIBLE TOILET	NEW VINYL ON EXISTING CONCRETE SLAB ON GRADE	NEW 4" COVE VINYL BASE	GYP BRD TO BE PAINTED	GYP BRD TO BE PAINTED	- - - -
102	MEN'S ACCESSIBLE TOILET	NEW VINYL ON EXISTING CONCRETE SLAB ON GRADE	NEW 4" COVE VINYL BASE	GYP BRD TO BE PAINTED	GYP BRD TO BE PAINTED	- - - -

GENERAL NOTES FOR INTERIOR FINISHES:

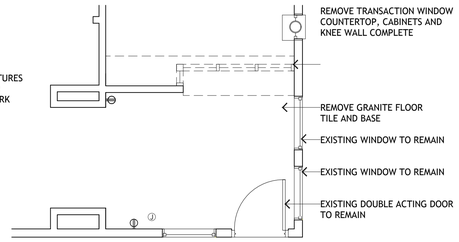
1 NEW GYP BRD REQUIRED IN ANY CONSTRUCTION TO HAVE TEXTURED FINISH TO MATCH EXISTING ADJACENT FINISH

2 ALL NEW PAINT COLORS TO BE SELECTED BY OWNER FROM STANDARD COLORS

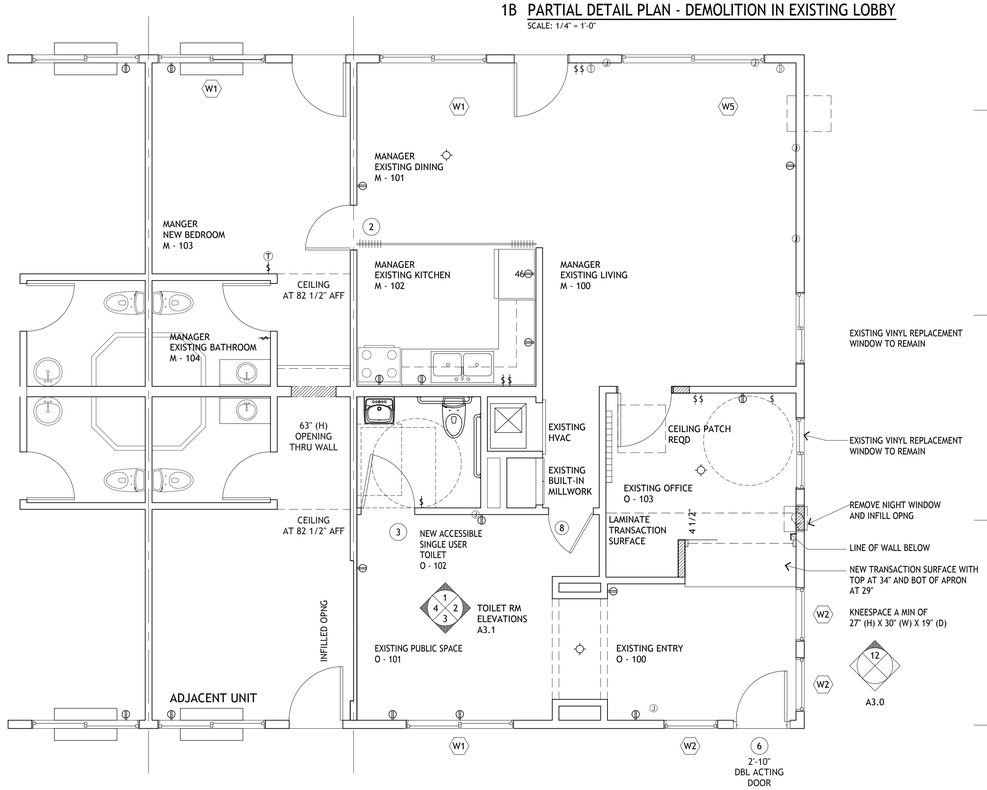
3 CONSTRUCTION CLEANING REQUIRED TO INCLUDE, BUT NOT BE LIMITED TO, TO THOROUGHLY CLEANING ANY EXISTING FIXTURES, FAUCETS, LIGHTING FIXTURES, ETC., TO BE REUSED IN NEW WORK.



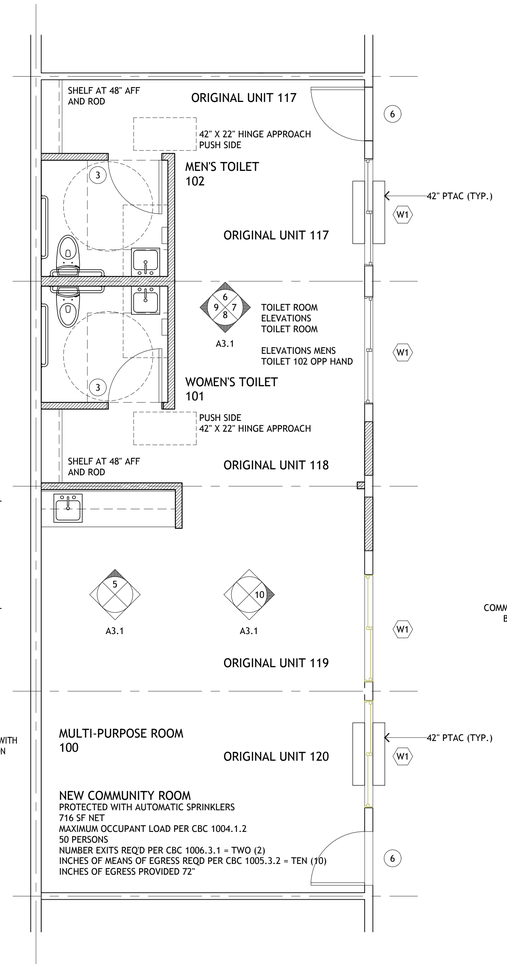
1A PARTIAL DETAIL PLAN
DEMOLITION IN EXISTING TOILET ROOM
SCALE: 1/4" = 1'-0"



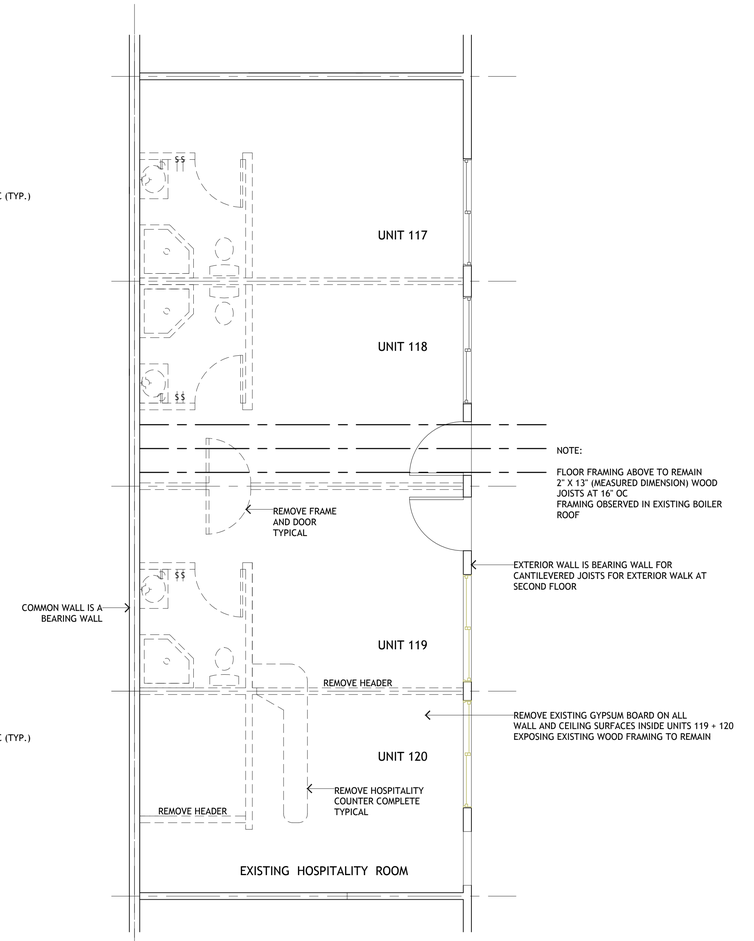
1B PARTIAL DETAIL PLAN - DEMOLITION IN EXISTING LOBBY
SCALE: 1/4" = 1'-0"



1 PARTIAL DETAIL REMODELED PLAN OF MANAGER'S UNIT
SCALE: 1/4" = 1'-0"



2 PARTIAL DETAIL REMODELED COMMUNITY ROOM PLAN
SCALE: 1/4" = 1'-0"



3 PARTIAL DETAIL DEMOLITION PLAN
SCALE: 1/4" = 1'-0"

BASIS Architecture & Consulting

2130 FOURTH ST
SAN RAFAEL, CA 94901
PHONE (415) 457-6035
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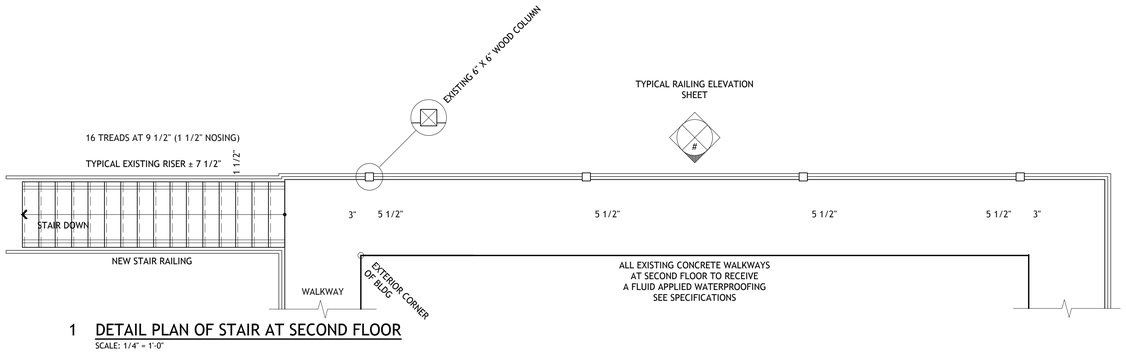
DETAIL
PLANS

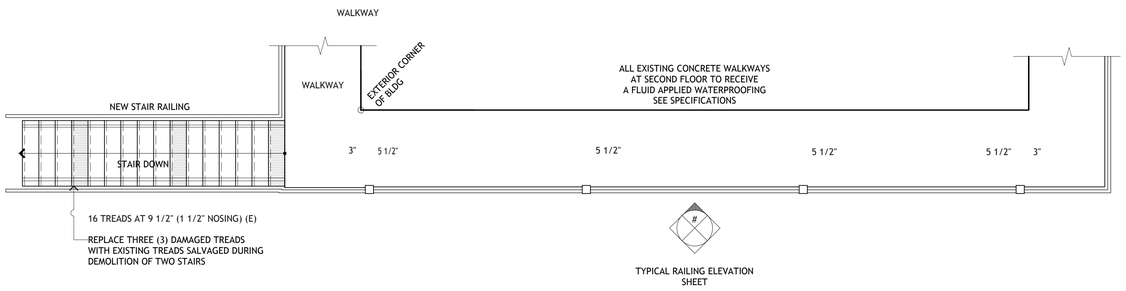
A1.5



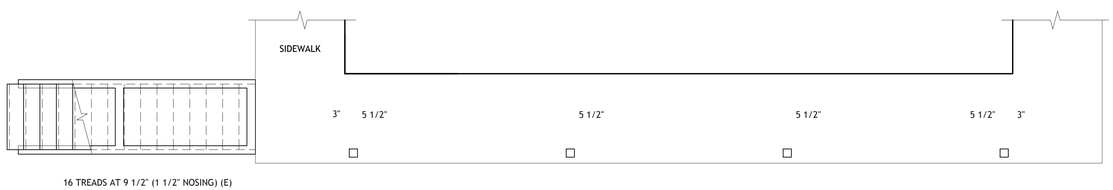
1 DETAIL PLAN OF STAIR AT SECOND FLOOR
SCALE: 1/4" = 1'-0"

NORTH

2 DETAIL PLAN OF STAIR AT GROUND FLOOR
SCALE: 1/4" = 1'-0"

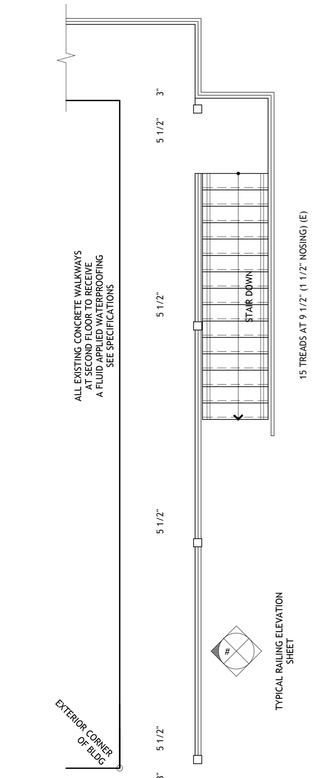


3 DETAIL PLAN OF STAIR AT SECOND FLOOR
SCALE: 1/4" = 1'-0"



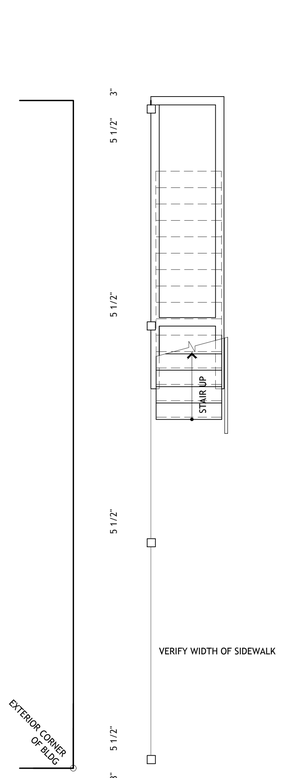
NORTH

4 DETAIL PLAN OF STAIR AT GROUND FLOOR
SCALE: 1/4" = 1'-0"



NORTH

5 DETAIL PLAN OF STAIR AT SECOND FLOOR
SCALE: 1/4" = 1'-0"



6 DETAIL PLAN OF STAIR GROUND FLOOR
SCALE: 1/4" = 1'-0"

BASIS Architecture & Consulting

2130 FOURTH ST
SAN RAFAEL, CA 94901
PHONE (415) 457-6035
FAX (415) 457-6036

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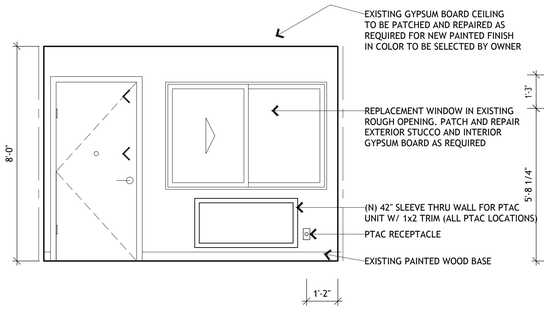
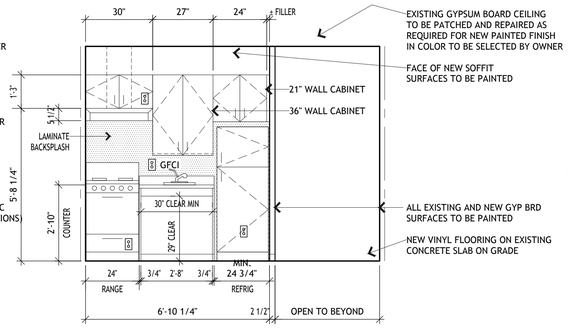
SHEET TITLE:

DETAIL
PLANS

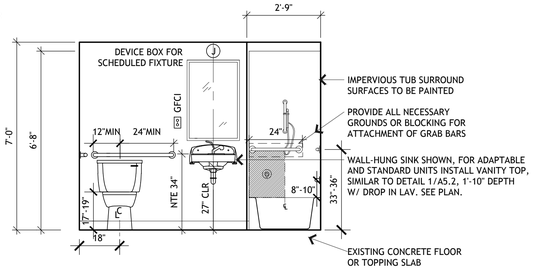
A1.6



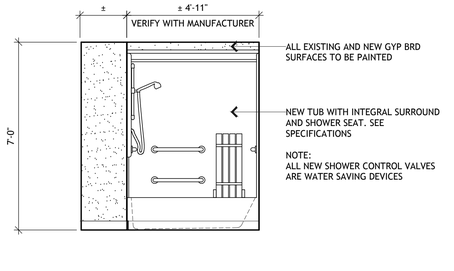
1 TYPICAL UNIT EXTERIOR WALL
SCALE: 3/8" =1'-0"



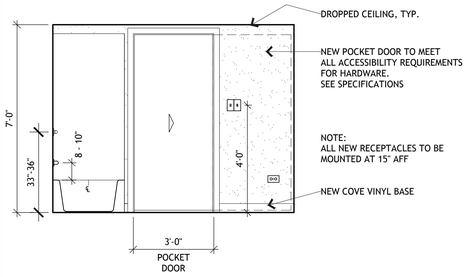
2 ACCESSIBLE IN-LINE UNIT KITCHEN ELEVATION
SCALE: 3/8" =1'-0"



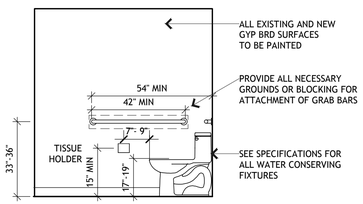
3 ACCESSIBLE IN-LINE UNIT BATHROOM ELEVATION
SCALE: 3/8" =1'-0"



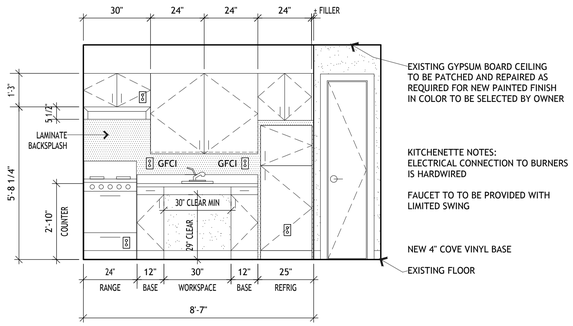
4 GROUND FLOOR ACCESSIBLE IN-LINE UNIT BATHROOM ELEVATION
SCALE: 3/8" =1'-0"



5 GROUND FLOOR ACCESSIBLE IN-LINE UNIT BATHROOM ELEVATION
SCALE: 3/8" =1'-0"



6 GROUND FLOOR ACCESSIBLE IN-LINE UNIT BATHROOM ELEVATION
SCALE: 3/8" =1'-0"



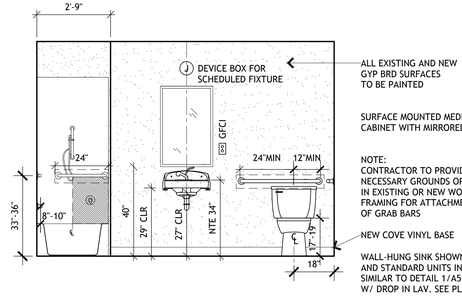
7 GROUND FLOOR ACCESSIBLE SIDE-BY-SIDE UNIT KITCHEN ELEVATION
SCALE: 3/8" =1'-0"

ACCESSIBILE/ADAPTABLE/STANDARD BATH NOTES:

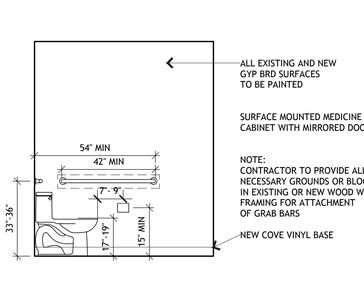
-ACCESSIBLE UNITS TO BE CONSTRUCTED AS DRAWN.

-"ADAPTABLE" UNITS SIMILAR, USE STANDARD TUB/SHOWER, DO NOT INSTALL GRAB BARS BUT DO PROVIDE BLOCKING FOR GRAB BARS (CONT. ¾" PLYWOOD BEHIND DRYWALL BTW. 15"-48" A.F.F., 3 SIDES TUB ENCLOSURE, BEHIND TOILET FROM CORNER TO 24", ALONGSIDE TOILET FROM CORNER TO 56", VANITY LAV.

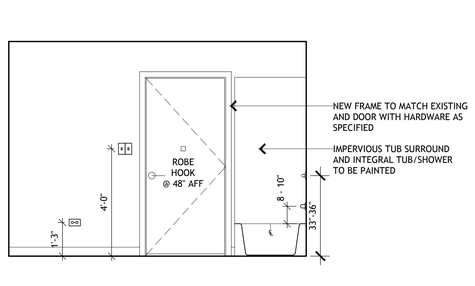
-STANDARD UNITS SIMILAR, NO GRAB BARS, STANDARD TUB/SHOWER, NO BLOCKING. VANITY LAV.



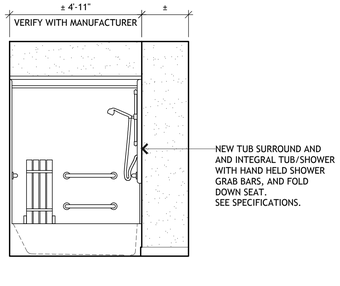
8 GROUND FLOOR ACCESSIBLE SIDE-BY-SIDE UNIT BATHROOM ELEVATION
SCALE: 3/8" =1'-0"



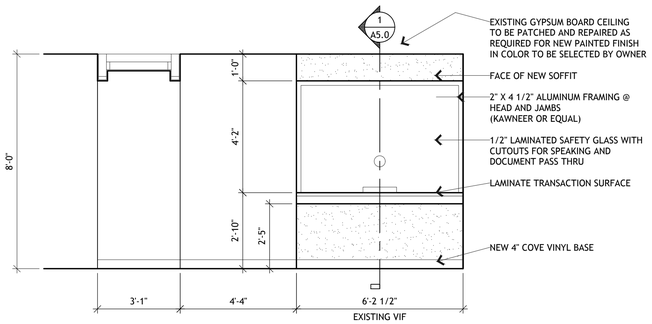
9 ACCESSIBLE SIDE-BY-SIDE UNIT BATHROOM ELEVATION
SCALE: 3/8" =1'-0"



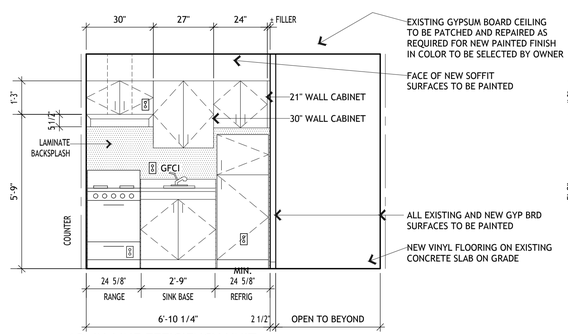
10 ACCESSIBLE SIDE-BY-SIDE UNIT BATHROOM ELEVATION
SCALE: 3/8" =1'-0"



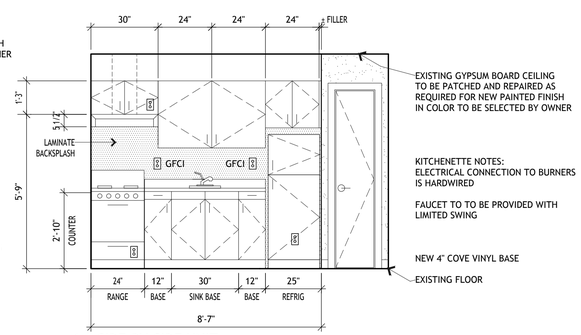
11 ACCESSIBLE SIDE-BY-SIDE UNIT BATHROOM ELEVATION
SCALE: 3/8" =1'-0"



12 OFFICE LOBBY
SCALE: 3/8" =1'-0"



13 IN-LINE EFFICIENCY UNIT KITCHEN ELEVATION
SCALE: 3/8" =1'-0"



14 SIDE-BY-SIDE UNIT KITCHEN ELEVATION
SCALE: 3/8" =1'-0"

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2130 FOURTH ST
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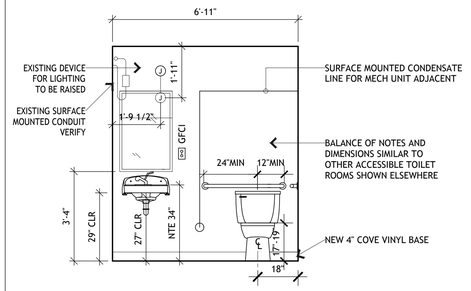
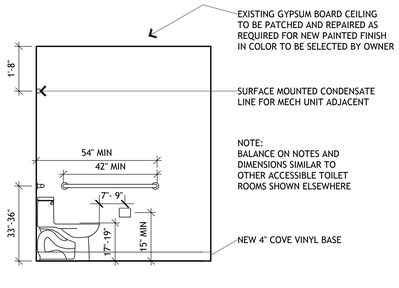
SHEET TITLE:

INTERIOR
ELEVATIONS

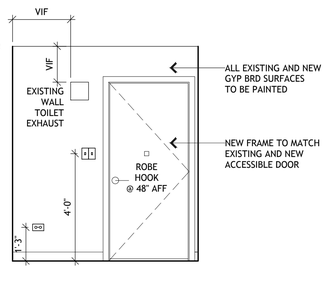
A3.0



1 ACCESSIBLE OFFICE SINGLE USER TOILET
SCALE: 3/4" =1'-0"



2 ACCESSIBLE OFFICE SINGLE USER TOILET
SCALE: 3/4" =1'-0"



3 ACCESSIBLE OFFICE SINGLE USER TOILET
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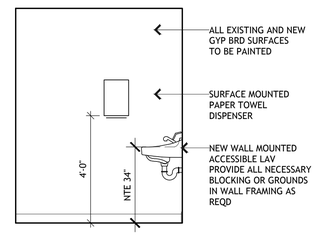

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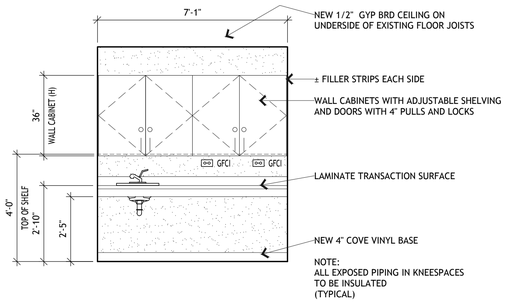
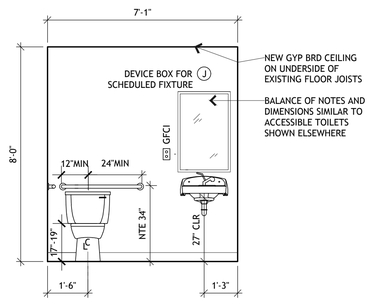
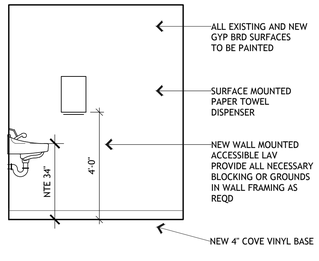
SCALE: 3/4" =1'-0"



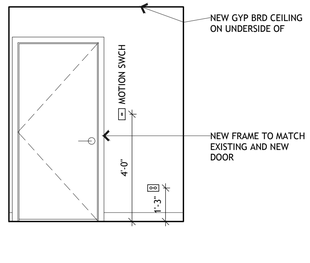
5 ACCESSIBLE COMMUNITY ROOM PANTRY
SCALE: 3/4" =1'-0"



6 COMMUNITY RM ACCESSIBLE TOILET
SCALE: 3/4" =1'-0"



7 COMMUNITY RM ACCESSIBLE TOILET
SCALE: 3/4" =1'-0"



8 COMMUNITY RM ACCESSIBLE TOILET
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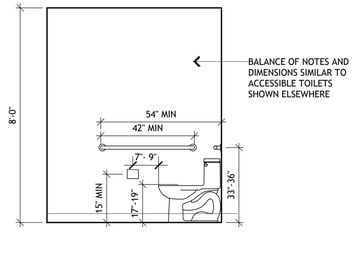
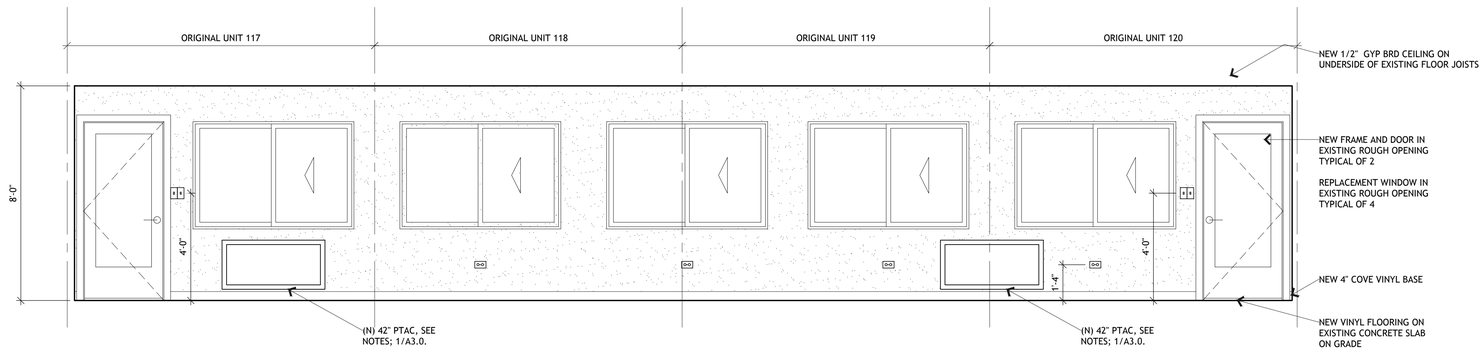
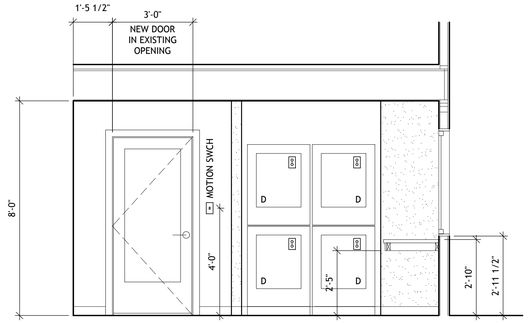


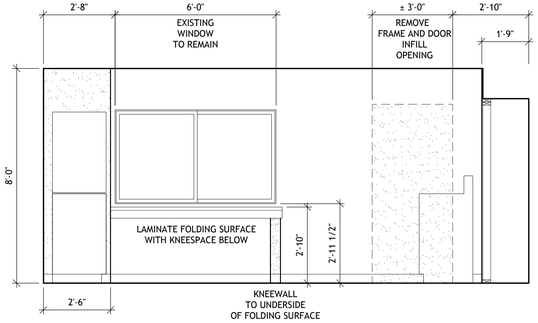
9 COMMUNITY RM ACCESSIBLE TOILET
SCALE: 3/4" =1'-0"



10 INTERIOR ELEVATION - COMMUNITY ROOM
SCALE: 3/4" =1'-0"



11 INTERIOR ELEVATION - COMMUNITY ROOM
SCALE: 3/4" =1'-0"



12 INTERIOR ELEVATION - REMODELED LAUNDRY
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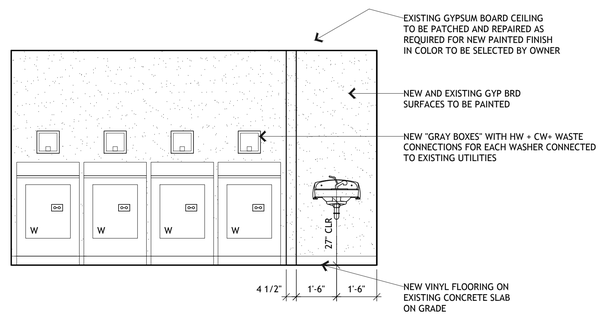


13 INTERIOR ELEVATION - REMODELED LAUNDRY
SCALE: 3/4" =1'-0"

BASIS Architecture & Consulting

2130 FOURTH ST
SAN RAFAEL, CA 94901
PHONE (415) 457-6035
FAX (415) 457-6036

P.O.BOX 150539
SAN RAFAEL, CA 94915

CHARLES PICK, ARCHITECT

LICENSED ARCHITECT
CHARLES PICK
C-21408
6-2019
RENEWAL DATE
STATE OF CALIFORNIA

DRAWING REVISION LOG

FOR BIO	5-15-2018

PROJECT NAME:

DESERT HAVEN
(QUEEN'S MOTEL)
RE-DEVELOPMENT

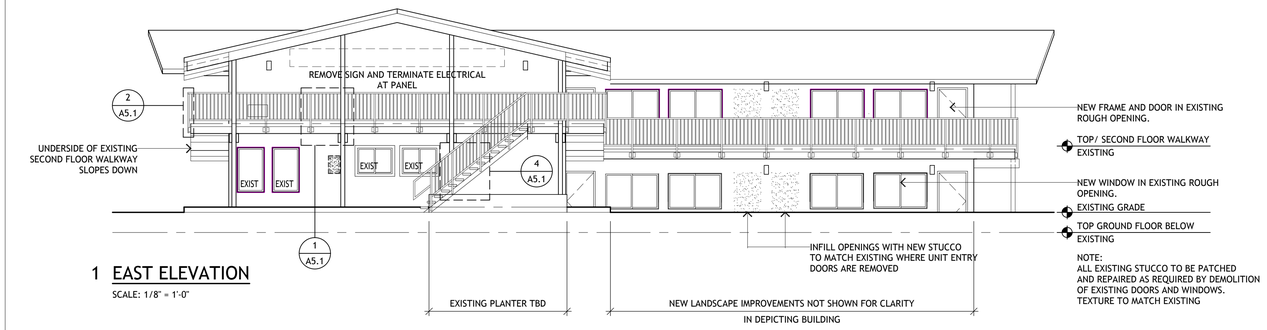
PROJECT LOCATION:

16959 STODDARD WELLS RI
VICTORVILLE, CA 92395

SHEET TITLE:

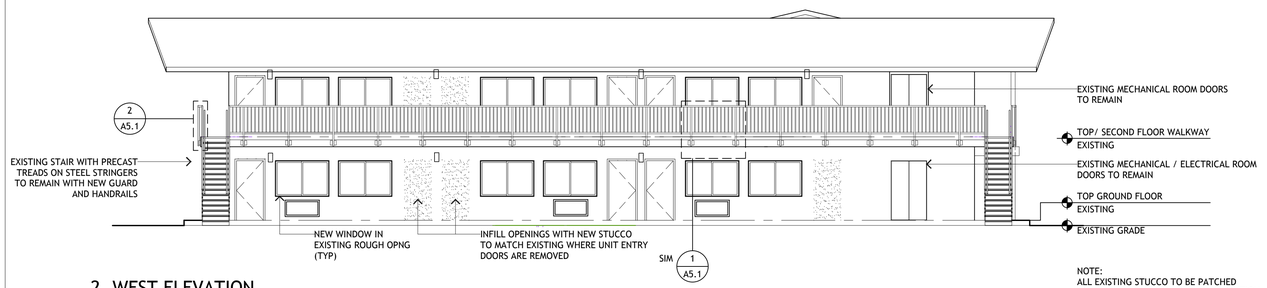
INTERIOR
ELEVATIONS

A3.1



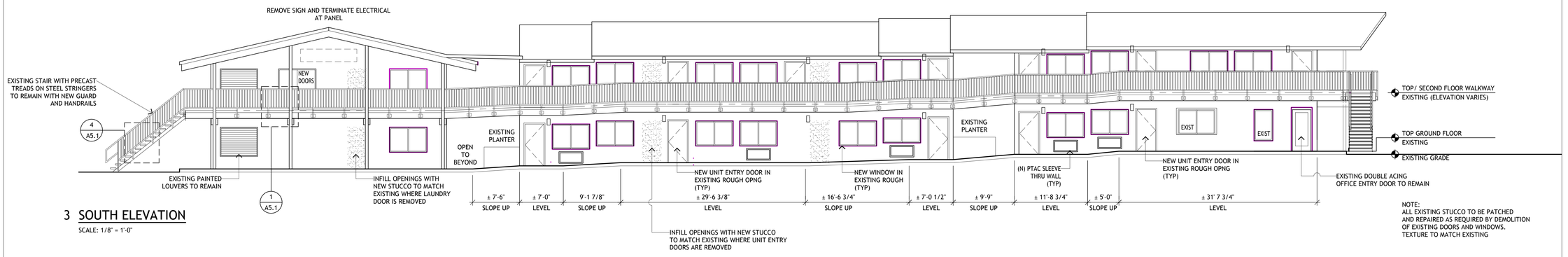
1 EAST ELEVATION

SCALE: 1/8" = 1'-0"



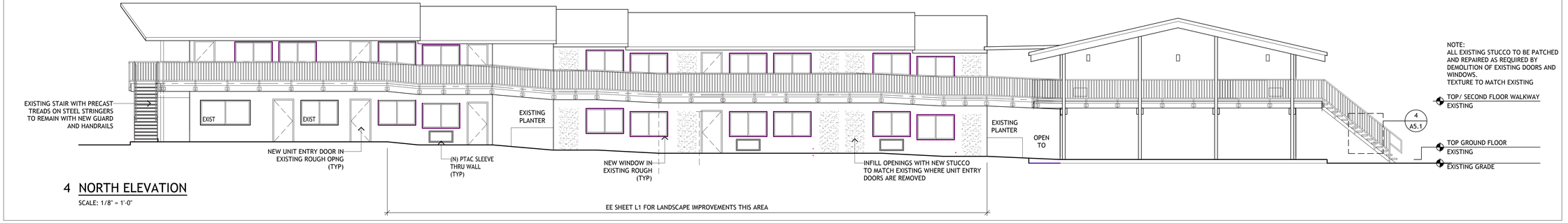
2 WEST ELEVATION

SCALE: 1/8" = 1'-0"



3 SOUTH ELEVATION

SCALE: 1/8" = 1'-0"



4 NORTH ELEVATION

SCALE: 1/8" = 1'-0"

BUILDING EXTERIOR GENERAL NOTES

1 STUCCO REPAIRS: ROUTE HAIRLINE CRACKS LESS THAN ⅛", FULLY SEAL, SAND FINISH TO MATCH EXISTING STUCCO. SEAL ALL.

2 SEAL ALL PENETRATIONS FOR PESTS AND RODENTS.

3 SEAL AND PAINT; APPLY WATERPROOFING CAULKING AT ALL JOINTS AND MATERIAL PENETRATIONS. PROPERLY PREPARED, PRIME, AND PAINT ALL WOOD ELEMENTS, TRIMS, SOFFITS, AND BARGE BOARDS.

4 EXTERIOR PAINT/ WATERPROOFING: APPLY NEW ACRYLIC TEXTURED TOPCOAT TO STUCCO AT WALLS. REMOVE EXISTING EXTERIOR CALKING AND REPLACE WITH NEW POLYURETHANE FOR WATERPROOFING INTEGRITY. UPON COMPLETION, PAINT ALL STUCCO SURFACES.

5 REPLACE ALL WINDOWS AT EXISTING ROUGH OPENINGS. COMPLY WITH EGRESS STANDARDS.

6 FASCIA REPAIR: INSPECT AND REPLACE/ DRY-ROTTED OR DAMAGED FASCIA THROUGHOUT. VERIFY CONDITIONS FOR DRY ROT, REPAIR AS NEEDED.

7 ROOF DRAINAGE: REPLACE ALL GUTTERS WITH FULL DEPTH FASCIA TYPE. REPLACE DOWNSPOUTS.

8 EXTERIOR KITCHEN AND BATH EXHAUST VENT HOODS: INTEGRATE WITH ROOFING REPLACEMENT.

9 BUILDING-MOUNTING LIGHTING: REPLACE ALL EXTERIOR LIGHTING WITH HIGH-EFFICIENCY TYPE.

10 CHECK ALL SOFFIT VENTS FOR AIRFLOW. BEAD BLAST OR OTHERWISE REMOVE PAINT WHERE IMPROPERLY APPLIED OVER VENT HOLES, PREVENTING PROPER OPERATION.

11 (N) RAILING

12 EXPAND (E) PTAC OPENING AND PROVIDE (N) 42" SLEEVE. SEE 10/A5.0.

13 LOCATIONS OF STUCCO INFILL @ EX-PTAC LOCATIONS NOT SHOWN.

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2130 FOURTH ST
SAN RAFAEL, CA 94901
PHONE (415) 457-6035
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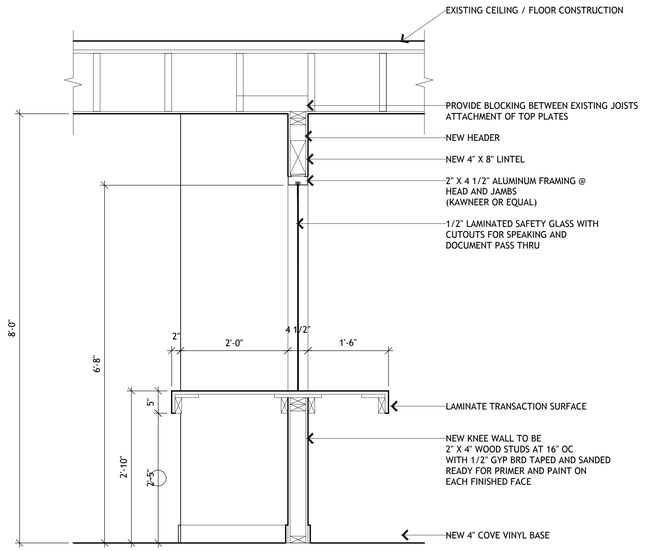
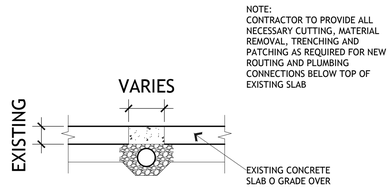
SHEET TITLE:

EXTERIOR
ELEVATIONS

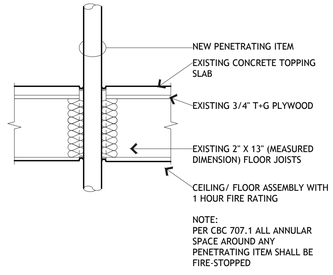
A4.0



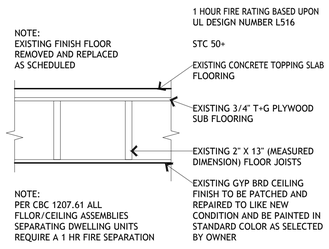
1 OFFICE TRANSACTION COUNTER
SCALE: 3/4" =1'-0"



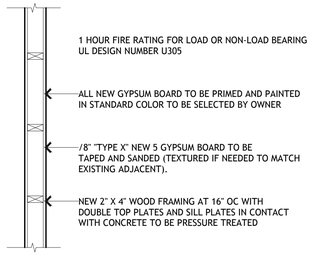
2 EXISTING CONCRETE FLOOR SLAB ON GRADE
1 HOUR RATED WITH STC 38
SCALE: 3/4" =1'-0"



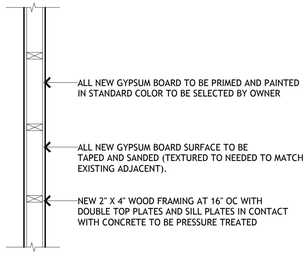
3 NEW PENETRATIONS OF EXISTING WOOD FRAMED
1 HR RATED CEILING/WALL ASSEMBLIES
SCALE: 3/4" =1'-0"



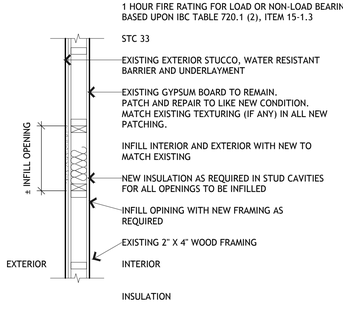
4 EXISTING WOOD FRAMED CEILING/FLOOR
1 HOUR RATED WITH STC 50
SCALE: 3/4" =1'-0"



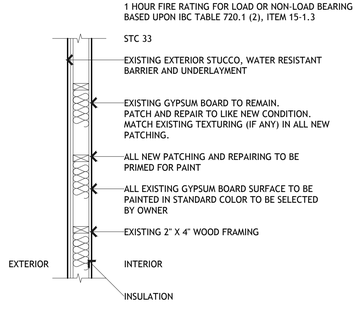
5 TYPICAL NEW BEARING OR NON-BEARING
INTERIOR PARTITION (1 HR RATED)
SCALE: 3/4" =1'-0"



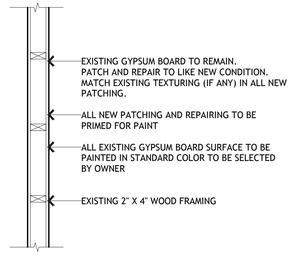
6 TYPICAL NEW BEARING OR NON-BEARING
INTERIOR PARTITION (NON-RATED)
SCALE: 3/4" =1'-0"



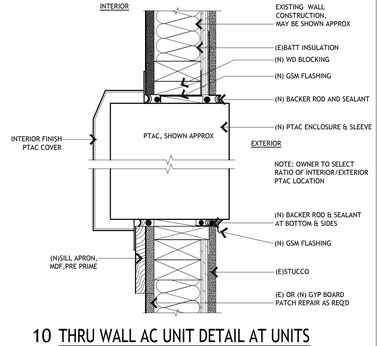
7 EXISTING EXTERIOR STUCCO WALL PATCHING
SCALE: 3/4" =1'-0"



8 TYPICAL EXISTING EXTERIOR STUCCO WALL
SCALE: 3/4" =1'-0"



9 TYPICAL EXISTING INTERIOR PARTITION
SCALE: 3/4" =1'-0"



10 THRU WALL AC UNIT DETAIL AT UNITS
SCALE: 1" =1'-0"

BASIS Architecture & Consulting

2130 FOURTH ST
SAN RAFAEL, CA 94901
PHONE (415) 457-6035
FAX (415) 457-6036

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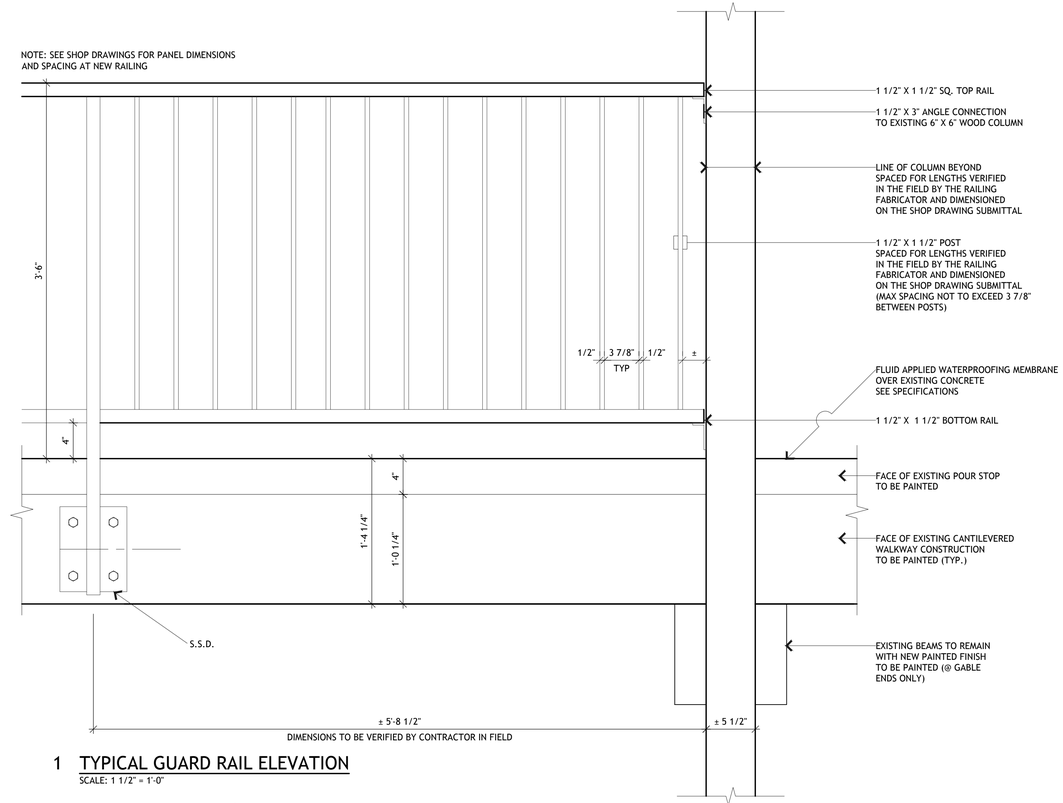
PROJECT LOCATION:

16959 STODDARD WELLS RI
VICTORVILLE, CA 92395

SHEET TITLE:

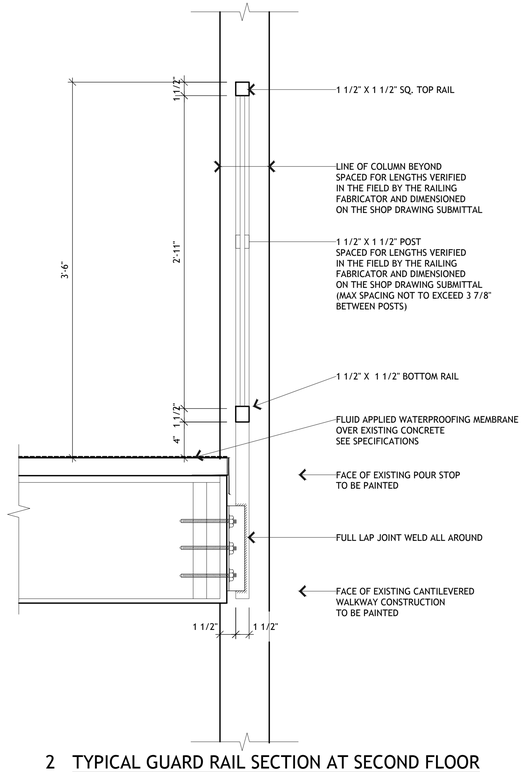
-
WALL DETAILS

A5.0



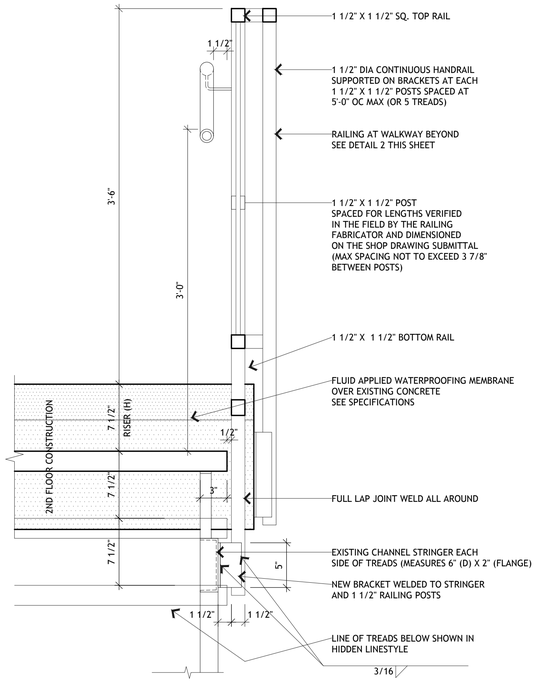
1 TYPICAL GUARD RAIL ELEVATION

SCALE: 1 1/2" = 1'-0"



2 TYPICAL GUARD RAIL SECTION AT SECOND FLOOR

SCALE: 1 1/2" = 1'-0"



3 TYPICAL GUARD RAIL SECTION AT EXISTING STAIRS

SCALE: 1 1/2" = 1'-0"

GENERAL NOTES FOR GUARDS AND RAILINGS

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LOADS ON HANDRAILS

CBC 1607.7.1 HANDRAILS AND GUARDS

HANDRAILS AND GUARDS SHALL BE DESIGNED TO RESIST A LOAD OF 50 POUNDS PER LINEAR FOOT (PLF) APPLIED IN ANY DIRECTION AT THE TOP AND TO TRANSFER THIS LOAD THROUGH THE SUPPORTS TO THE STRUCTURE.

CBC 1607.7.1.1 CONCENTRATED LOAD
HANDRAILS AND GUARDS SHALL BE ABLE TO RESIST A SINGLE CONCENTRATED LOAD OF 200 POUNDS, APPLIED IN ANY DIRECTION AT ANY POINT ALONG THE TOP, AND TO TRANSFER THIS LOADS THROUGH THE SUPPORTS TO THE STRUCTURE. THIS LOAD NEED NOT BE ASSUMED TO ACT CONCURRENTLY WITH THE LOADS SPECIFIED IN 1607.7.1.

CBC 1607.7.1.2 COMPONENTS
INTERMEDIATE RAILS (ALL THIS EXCEPT THE HANDRAIL, BALLUSTERS AND PANEL FILLERS SHALL BE DESIGNED TO WITHSTAND A HORIZONTALLY APPLIED NORMAL LOAD OF 50 POUNDS ON AN AREA EQUAL TO 1 SQUARE FOOT, INCLUDING OPENINGS AND SPACE BETWEEN RAILS. REACTIONS DUE TO THIS LOADING ARE NOT REQUIRED TO BE SUPERIMPOSED WITH THOSE OF SECTION1607.7.1 OR 1607.7.1.1.

HANDRAILS

CBC 1012.2 HEIGHT
HANDRAIL HEIGHT, MEASURED ABOVE STAIR TREAD NOSING, OR FINISH SURFACE OF RAMP SLOPE, SHALL BE UNIFORM, NOT LESS THAN 34 INCHES AND NOT MORE THAN 38 INCHES.

CBC 1012.3 HANDRAIL GRASPABILITY

CBC 1012.3.1 TYPE I HANDRAILS.
HANDRAILS WITH A CIRCULAR CROSS SECTION SHALL HAVE AN OUTSIDE DIAMETER OF AT LEAST 1 ¼ INCHES AND NOT GREATER THAN 2 INCHES. IF THE HANDRAIL IS NOT CIRCULAR, IT SHALL HAVE A PERIMETER DIMENSION OF AT LEAST 4 INCHES AND NOT GREATER THAN 6 ¼ INCHES WITH A MAXIMUM CROSS-SECTION DIMENSION OF 2 ¼ INCHES. EDGES SHALL HAVE A MINIMUM RADIUS OF 0.01 INCH.

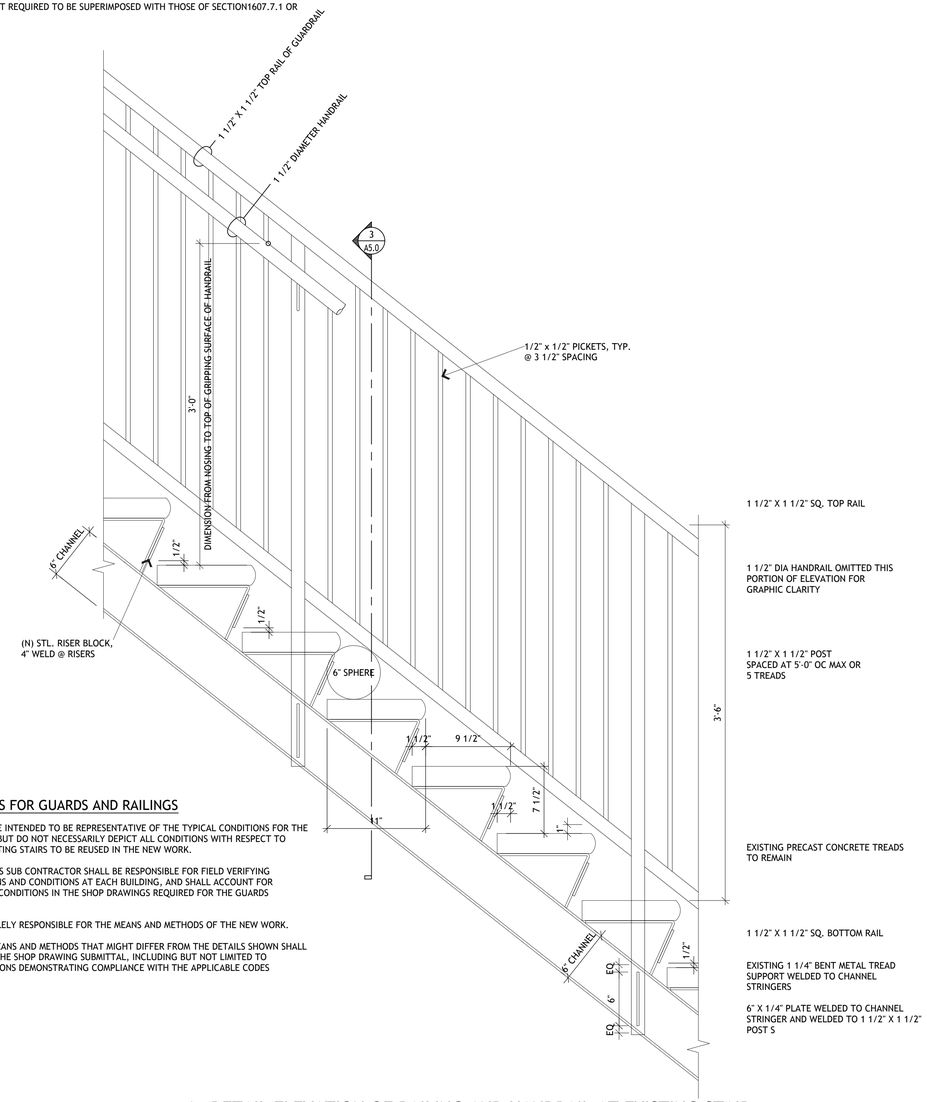
CBC 1012.4 CONTINUITY
HANDRAIL GRIPPING SURFACES SHALL BE CONTINUOUS, WITHOUT INTERRUPTION…

CBC 1013.2 (REQUIRED HEIGHT OF) GUARDS
REQUIRED GUARDS SHALL BE NOT LESS THAN 42 INCHES HIGH, MEASURED VERTICALLY ABOVE THE ADJACENT WALKING SURFACES.

CBC 1013.3 OPENING LIMITATIONS
REQUIRED GUARDS SHALL NOT HAVE OPENINGS WHICH ALLOW PASSAGE OF A SPHERE 4 INCHES IN DIAMETER FROM THE WALKING SURFACE TO THE REQUIRED GUARD HEIGHT.

EXCEPTIONS:
FROM A HEIGHT OF 36 INCHES TO 42 INCHES, GUARDS SHALL NOT HAVE OPENINGS WHICH ALLOW PASSAGE OF A SPHERE 4 3/8 INCHES IN DIAMETER.

THE TRIANGULAR OPENINGS AT THE OPEN SIDE OF A STAIR, FORMED BY THE RISER, TREAD AND BOTTOM RAIL SHALL NOT ALLOW PASSAGE OF A SPHERE 6 INCHES IN DIAMETER.



4 DETAIL ELEVATION OF RAILING AND HANDRAIL AT EXISTING STAIR

SCALE: 1 1/2" = 1'-0"

CONTRACTOR NOTES FOR GUARDS AND RAILINGS

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RAILING
DETAILS

A5.1

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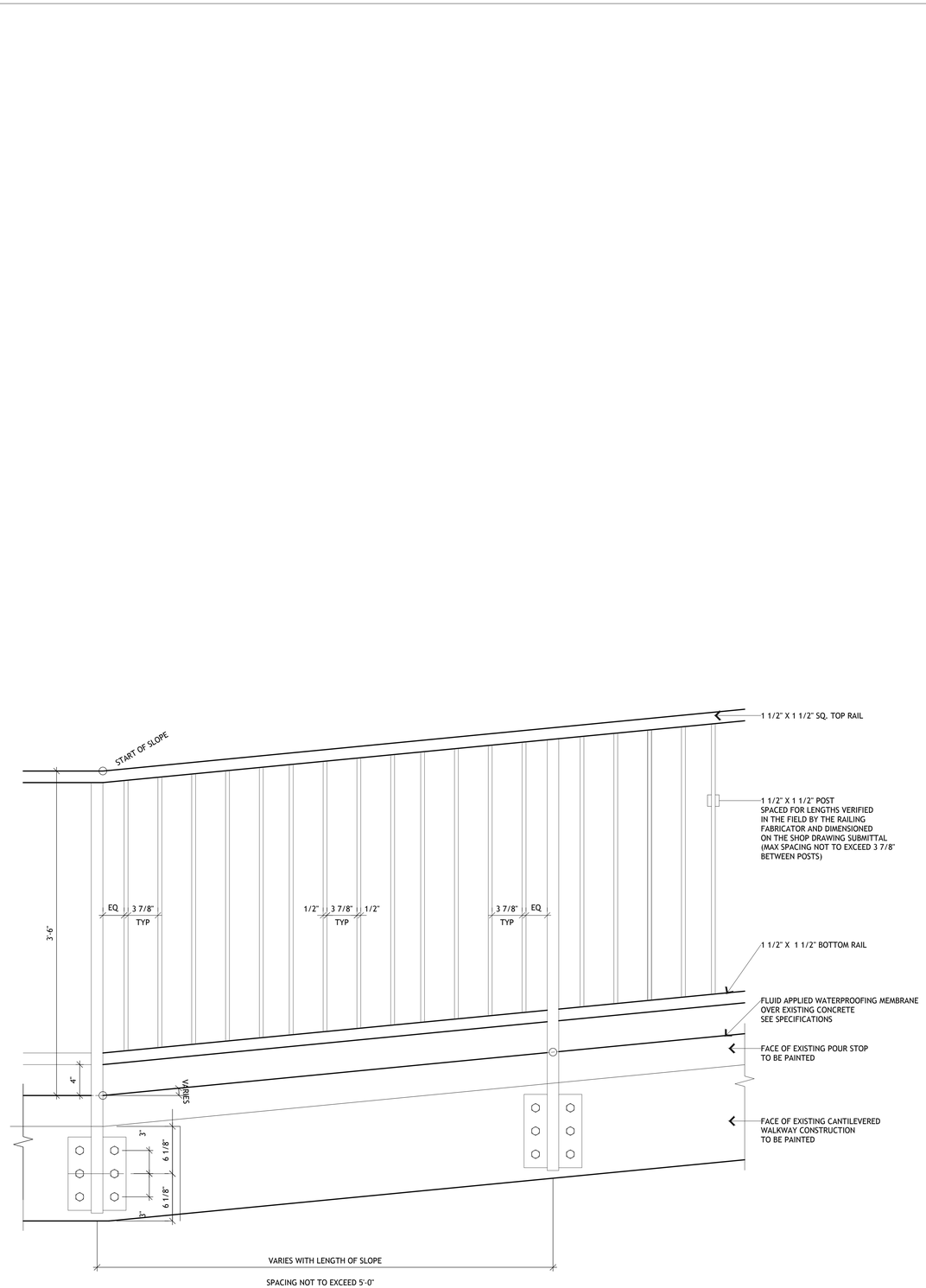
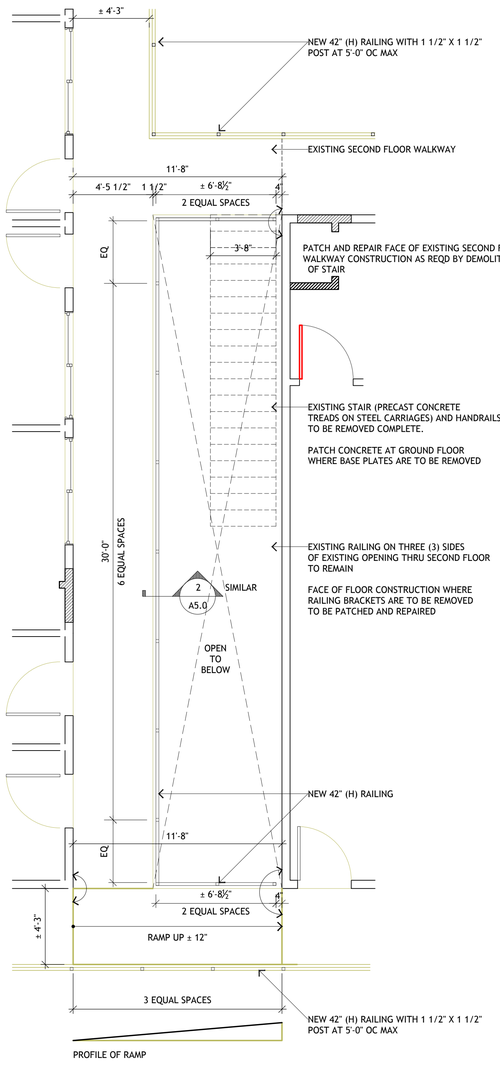
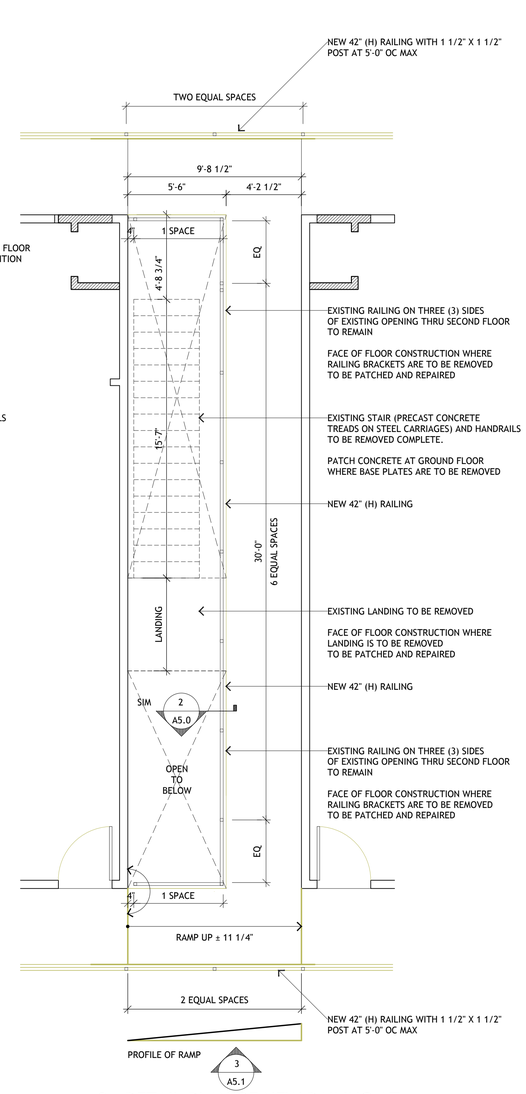
CONDITIONS FOR THE MEANS AND METHODS THAT MIGHT DIFFER FROM THE DETAILS SHOWN SHALL BE ACCOUNTED FOR IN THE SHOP DRAWING SUBMITTAL.



3 DETAIL ELEVATION OF RAILING AT EXISTING SLOPING SECOND FLOOR WALKWAY
SCALE: 1 1/2" = 1'-0"



1 DETAIL PLAN OF NEW RAILING AT EXISTING SECOND FLOOR OPENING
SCALE: 14" = 1'-0"



2 DETAIL PLAN OF NEW RAILING AT EXISTING SECOND FLOOR OPENING
SCALE: 14" = 1'-0"

BASIS Architecture & Consulting

2130 FOURTH ST
SAN RAFAEL, CA 94901
PHONE (415) 457-6035
FAX (415) 457-6036

P.O.BOX 150539
SAN RAFAEL, CA 94915

CHARLES PICK, ARCHITECT

LICENSED ARCHITECT
CHARLES PICK
C-21406
6-2019
RENEWAL DATE
STATE OF CALIFORNIA

DRAWING REVISION LOG

FOR BIO	5-15-2018

PROJECT NAME:

DESERT HAVEN
(QUEEN'S MOTEL)
RE-DEVELOPMENT

PROJECT LOCATION:

16959 STODDARD WELLS RI
VICTORVILLE, CA 92395

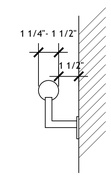
SHEET TITLE:

-
RAILING
DETAILS

A5.2

NOTES:
HANDRAILS ARE REQUIRED ON PEDESTRIAN RAMPS THAT PROVIDE ACCESS, EXCEPT THAT AT EXTERIOR DOOR LANDINGS, HANDRAILS ARE NOT REQUIRED ON RAMPS LESS THAN 6" RISE OR 72" IN LENGTH.

(1) HANDRAILS ARE ON BOTH SIDES AND CONTINUOUS THE FULL LENGTH OF THE RAMP.
(2) FINISH HEIGHT OF HANDRAILS 34"-38" ABOVE RAMP SURFACE.
(3) HANDRAILS EXTEND A MIN. OF 12" OF LEVEL DISTANCE BEYOND TOP AND BOTTOM OF THE RAMP AND ARE RETURNED.
(4) HANDRAIL HAS SMOOTH SURFACE, NO SHARP EDGES OR CORNERS, AND EDGES HAVE A MIN. RADIUS OF $\frac{1}{8}$".
(5) HANDRAIL HAS ENOUGH MIN. STRUCTURAL STRENGTH TO SUPPORT A 250-LB. POINT LOAD.
(6) HANDRAILS DO NOT ROTATE WITHIN THEIR FITTINGS.
(7) WALL OR OTHER SURFACE ADJACENT TO THE HANDRAIL IS FREE OF SHARP OR ABRASIVE ELEMENTS.
(8) ENDS OF HANDRAILS ARE EITHER ROUNDED OR RETURNED SMOOTHLY TO FLOOR, WALL, OR OTHER POST.



ENLARGED DETAIL (NTS)

7 HANDRAILS
SCALE: 1/2" =1'-0"

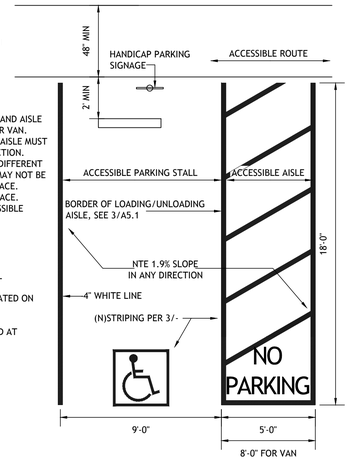
SEE 5/A5.1 CURB-CUT RAMPS:
ADDITIONAL REQUIREMENTS FOR TRANSITION FROM ACCESSIBLE AISLE TO ROUTE.

ACCESSIBLE PARKING SPACE DESIGN

1. STANDARD STALL MUST BE 9'-0" BY 18'-0" AND AISLE MUST BE 5'-0" BY 18'-0" AND 8'-0" WIDE FOR VAN.
2. SURFACE OF PARKING SPACE AND ACCESS AISLE MUST NOT EXCEED 1.9% GRADIENT IN ANY DIRECTION.
3. CURB RAMP REQUIRED WHEN WALK IS AT DIFFERENT LEVEL THAN PARKING ELEVATION. RAMP MAY NOT BE LOCATED IN ACCESS AISLE OR PARKING SPACE.
4. PROVIDE PARKING BUMPER OR CURB IN SPACE.
5. PROVIDE THE CORRECT NUMBER OF ACCESSIBLE STALLS ON SITE.

VAN ACCESSIBLE STALL

IN ADDITION TO STANDARD ACCESSIBLE STALL REQUIREMENTS:
1. ACCESSIBLE AISLE MUST BE 8'-0" AND LOCATED ON PASSENGER SIDE.
2. "VAN ACCESSIBLE" SIGNAGE IS REQUIRED.
3. 98" MIN VERTICAL CLEARANCE IS PROVIDED AT PARKING SPACE AND VEHICLE ROUTE.



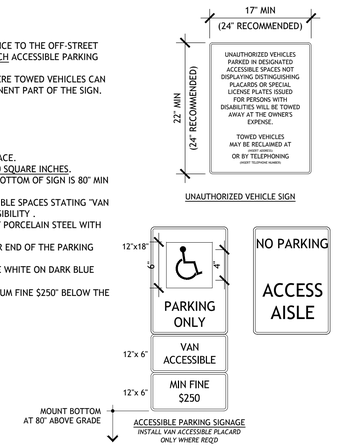
4 ACCESSIBLE PARKING SPACE DESIGN
SCALE: 1/4" =1'-0"

AUTHORIZED VEHICLE WARNING SIGN

1. SIGN IS TO BE POSTED AT EACH ENTRANCE TO THE OFF-STREET PARKING FACILITIES, OR POSTED AT EACH ACCESSIBLE PARKING STALL.
2. THE PHONE NUMBER AND ADDRESS WHERE TOWED VEHICLES CAN BE RECLAIMED IS POSTED AND A PERMANENT PART OF THE SIGN.
3. LETTERS MUST BE 1" MIN IN HEIGHT.
4. SIGN IS NOT LESS THAN 17" BY 22".

PARKING SPACE SIGNAGE

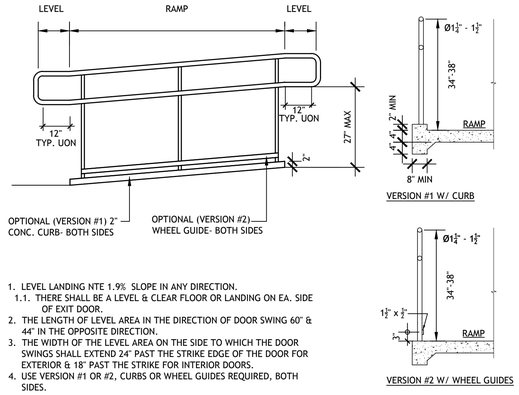
1. POST SIGNAGE AT EACH ACCESSIBLE SPACE.
2. AREA OF SIGN IS NOT SMALLER THAN 70 SQUARE INCHES.
3. WHEN POSTED IN A PATH OF TRAVEL, BOTTOM OF SIGN IS 80" MIN FROM FINISHED GRADE.
4. POST ADDITIONAL SIGN AT VAN ACCESSIBLE SPACES STATING "VAN ACCESSIBLE" BELOW SYMBOL OF ACCESSIBILITY .
5. REFLECTORIZED SIGN CONSTRUCTED OF PORCELAIN STEEL WITH BEADED TEXT OR EQUAL.
6. SIGN TO BE CENTERED AT THE INTERIOR END OF THE PARKING SPACE.
7. TEXT AND ACCESSIBILITY SYMBOL TO BE WHITE ON DARK BLUE BACKGROUND.
8. POST ADDITIONAL SIGN STATING "MINIMUM FINE $250" BELOW THE SYMBOL OF ACCESSIBILITY.



UNAUTHORIZED VEHICLE SIGN

ACCESSIBLE PARKING SIGNAGE
INSTALL VAN ACCESSIBLE PLACARD ONLY WHERE REQ'D

1 ACCESSIBLE SIGNAGE AT PARKING AREAS
SCALE: 1 1/2" =1'-0"

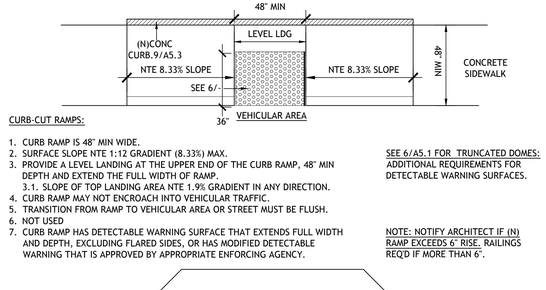


VERSION #1 W/ CURB

VERSION #2 W/ WHEEL GUIDES

1. LEVEL LANDING NTE 1.9% SLOPE IN ANY DIRECTION.
   1.1. THERE SHALL BE A LEVEL & CLEAR FLOOR OR LANDING ON EA. SIDE OF EXIT DOOR.
2. THE LENGTH OF LEVEL AREA IN THE DIRECTION OF DOOR SWING 60" & 44" IN THE OPPOSITE DIRECTION.
3. THE WIDTH OF THE LEVEL AREA ON THE SIDE TO WHICH THE DOOR SWINGS SHALL EXTEND 24" PAST THE STRIKE EDGE OF THE DOOR FOR EXTERIOR & 18" PAST THE STRIKE FOR INTERIOR DOORS.
4. USE VERSION #1 OR #2, CURBS OR WHEEL GUIDES REQUIRED, BOTH SIDES.

8 PEDESTRIAN RAMPS
SCALE: 1/2" =1'-0"

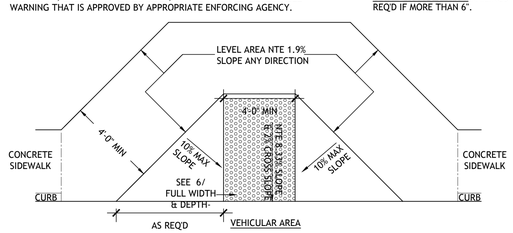


CURB-CUT RAMPS:

1. CURB RAMP IS 48" MIN WIDE.
2. SURFACE SLOPE NTE 1:12 GRADIENT (8.33%) MAX.
3. PROVIDE A LEVEL LANDING AT THE UPPER END OF THE CURB RAMP, 48" MIN DEPTH AND EXTEND THE FULL WIDTH OF RAMP.
   3.1. SLOPE OF TOP LANDING AREA NTE 1.9% GRADIENT IN ANY DIRECTION.
4. CURB RAMP MAY NOT ENCROACH INTO VEHICULAR TRAFFIC.
5. TRANSITION FROM RAMP TO VEHICULAR AREA OR STREET MUST BE FLUSH.
6. NOT USED
7. CURB RAMP HAS DETECTABLE WARNING SURFACE THAT EXTENDS FULL WIDTH AND DEPTH, EXCLUDING FLARED SIDES, OR HAS MODIFIED DETECTABLE WARNING THAT IS APPROVED BY APPROPRIATE ENFORCING AGENCY.

SEE 6/A5.1 FOR TRUNCATED DOMES:
ADDITIONAL REQUIREMENTS FOR DETECTABLE WARNING SURFACES.

NOTE: NOTIFY ARCHITECT IF (N) RAMP EXCEEDS 6" RISE. RAILINGS REQ'D IF MORE THAN 6".



5 CURB-CUT RAMPS
SCALE: 1/4" =1'-0"

ACCESSIBLE ROUTE OF TRAVEL

1. AN ACCESSIBLE ROUTE IS PROVIDED TO THE COMMON BUILDING ENTRANCE, ACCESSIBLE PARKING SPACES, AND PUBLIC STREET.

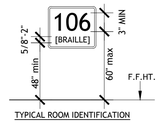
SIGNS AND IDENTIFICATION

1. SIGNAGE CONTAINING THE INTERNATIONAL SYMBOL OF ACCESSIBILITY IS LOCATED AT EVERY PRIMARY PUBLIC ENTRANCE AND AT EVERY MAJOR EXTERIOR JUNCTION WHERE ACCESSIBLE ROUTE DIVERGES FROM THE REGULAR CIRCULATION PATH.
2. SIGNAGE INDICATES THE DIRECTION TO ACCESSIBLE BUILDING ENTRANCES.
3. CHARACTERS AND SYMBOLS MUST CONTRAST WITH THEIR BACKGROUND. WHITE TEXT ON BLUE BACKGROUND RECOMMENCED. (LIGHT TEXT ON DARK BACKGROUND OR DARK ON LIGHT ACCEPTABLE.)
4. CHARACTERS HAVE A WIDTH-TO-HEIGHT RATIO BETWEEN 3:5 AND 1:1 AND A STROKE WIDTH-TO-HEIGHT RATIO BETWEEN 1:5 AND 1:1.
5. AT DIRECTIONAL SIGNAGE TO BUILDINGS OR FACILITIES, CHARACTERS ARE A MINIMUM OF 3" IN HT. WHEN POSTED OVERHEAD 80" OR MORE.
6. SIGNAGE IS TO BE INSPECTED AND APPROVED BY THE ENFORCING AGENCY THROUGH PLAN REVIEW AND INSPECTED AFTER INSTALLATION.

AT PERMANENT ROOMS AND SPACES:

1. LETTERS AND NUMERALS ON SIGNS ARE RAISED $\frac{1}{32}$", UPPERCASE CHARACTERS 3" MIN HEIGHT, AND WRITTEN IN BRAILLE DIRECTLY BELOW TEXT, $\frac{3}{8}$"-2" HEIGHT.
2. MOUNT SIGNS ADJACENT TO LATCH SIDE OF DOOR.
3. SIGNAGE IS TO BE INSPECTED AND APPROVED BY THE ENFORCING AGENCY THROUGH PLAN REVIEW AND INSPECTED AFTER INSTALLATION.

TYPICAL DIRECTIONAL SIGNAGE
(DIRECTION WILL VARY)



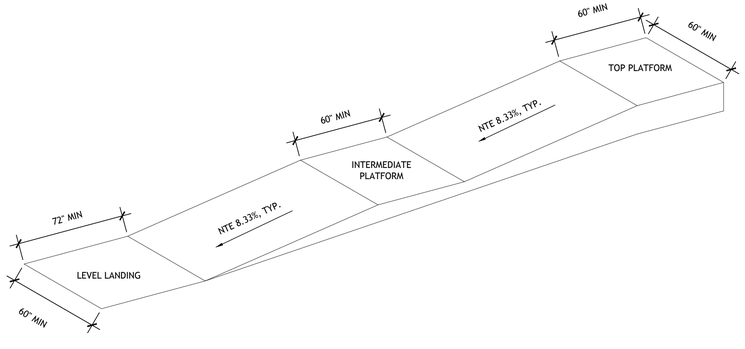
TYPICAL ROOM IDENTIFICATION

2 ACCESSIBLE ROUTE OF TRAVEL AND SIGNAGE
SCALE: 1 1/2" =1'-0"

NOTES:
GENERAL: ANY PATH OF TRAVEL IS CONSIDERED A RAMP IF ITS SLOPE IS GREATER THAN 1:20 OR 4.9%. THE LEAST POSSIBLE SLOPE SHALL BE USED FOR ANY RAMP.
HANDRAILS ARE REQUIRED ON PEDESTRIAN RAMPS THAT PROVIDE ACCESS, EXCEPT THAT AT EXTERIOR DOOR LANDINGS, HANDRAILS ARE NOT REQUIRED ON RAMPS LESS THAN 6" RISE OR 72" IN LENGTH.

(1) BOTTOM AND INTERMEDIATE LANDING(S) ARE AT LEAST AS WIDE AS THE REQUIRED RAMP WIDTH.
(2) SURFACE SLOPE OF LANDING(S) DOES NOT EXCEED 1:50 GRADIENT (2%) IN ANY DIRECTION.
(3) INTERMEDIATE LANDING(S) ARE A MIN. OF 60" LONG IN THE RAMP DIRECTION.
(4) INTERMEDIATE LANDING(S) WHERE THE RAMP CHANGES DIRECTION ARE 60" BY 60" MIN.
(5) FINISHED SURFACE OF RAMP AND LANDING(S) IS STABLE, FIRM AND SLIP-RESISTANT.
(6) RAMPS AND LANDINGS WITH DROP-OFFS HAVE EDGE PROTECTION CONSISTING OF EITHER A GUIDE CURB A MIN. 2" IN HEIGHT ON EACH SIDE OF THE RAMP, OR A WHEEL GUIDE RAIL CENTERED 3" PLUS OR MINUS 1" ON EACH SIDE OF THE RAMP.
(7) OUTDOOR RAMP(S) AND THEIR APPROACHES ARE DESIGNED SO WATER DOES NOT ACCUMULATE ON WALKING SURFACES.

SEE 7/A5.1 PEDESTRIAN RAMPS AND 8/A5.1 HANDRAILS:
ADDITIONAL REQUIREMENTS FOR RAMPS



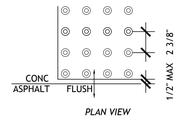
9 GENERAL RAMP
SCALE: 1/4" =1'-0"

DETECTABLE WARNING SURFACE: TRUNCATED DOMES

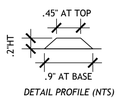
1. DETECTABLE WARNING SURFACE EXTENDS THE FULL WIDTH AND DEPTH OF A CURB-CUT RAMP, EXCLUDING THE FLARED SIDES, IF IT ADJOINS A VEHICULAR TRAFFIC WAY.
2. DETECTABLE WARNING SURFACE CONTRASTS VISUALLY WITH ADJOINING SURFACES, EITHER LIGHT ON DARK OR DARK ON LIGHT.
3. DETECTABLE WARNING SURFACE DIFFERS FROM ADJOINING WALKING SURFACES IN RESILIENCY OR SOUND ON CANE CONTACT.

MODIFIED DETECTABLE WARNING SURFACE:

1. PATTERN TO BE STAMPED IN CONCRETE. PROFILE DIMENSIONS MATCH THAT OF TRUNCATED DOMES.
2. PAINT DETECTABLE WARNING SURFACE BLACK TO CONTRAST VISUALLY WITH ADJOINING SURFACES.



PLAN VIEW

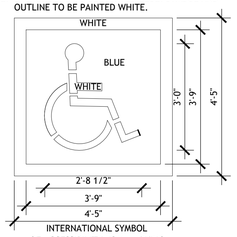


DETAIL PROFILE (NTS)

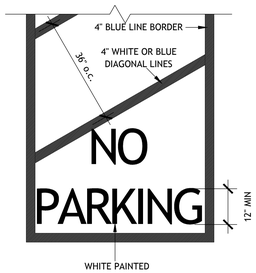
6 GROOVED BORDER & DETECTABLE WARNING
SCALE: 1 1/2" =1'-0"

INTERNATIONAL SYMBOL OF ACCESSIBILITY:

1. SYMBOL DEPICTS PROFILE VIEW OF WHEELCHAIR WITH OCCUPANT.
2. BACKGROUND TO BE PAINTED BLUE. SYMBOL AND OUTLINE TO BE PAINTED WHITE.



INTERNATIONAL SYMBOL OF ACCESSIBILITY (ISA) MARKING



WHITE PAINTED

NOTES:
1. THE DESIGN DETAILS FOR THIS SYMBOL, LEGENDS AND RELATED MARKINGS ARE SHOWN IN THE DEPARTMENT OF TRANSPORTATION STANDARD PLANS. SEE STANDARD PLAN 24C FOR SQUARE UNIT AREA FOR ISA MARKING.
2. THE WORDS "NO PARKING" SHALL BE PAINTED IN THE LOADING AND UNLOADING AREA IN WHITE LETTERS NO LESS THAN 12" IN HEIGHT ON A CONTRASTING BACKGROUND AND LOCATED SO THAT IT IS VISABLE TO TRAFFIC ENFORCEMENT OFFICIALS. SEE STANDARD PLAN A24E FOR SQUARE UNIT AREA FOR "NO PARKING" LEGEND.
3. LOADING AND UNLOADING AREA BORDER SHALL BE MARKED IN BLUE PAINT.

SEE MUTCD 2014 EDITION

3 ACCESSIBLE PARKING SPACE STRIPING
SCALE: 1 1/2" =1'-0"

BASIS Architecture & Consulting

2130 FOURTH ST
SAN RAFAEL, CA 94901
PHONE (415) 457-6035
FAX (415) 457-6036

P.O.BOX 150539
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FOR BIO	5-15-2018

PROJECT NAME:

DESERT HAVEN
(QUEEN'S MOTEL)
RE-DEVELOPMENT

PROJECT LOCATION:

16959 STODDARD WELLS RI
VICTORVILLE, CA 92395

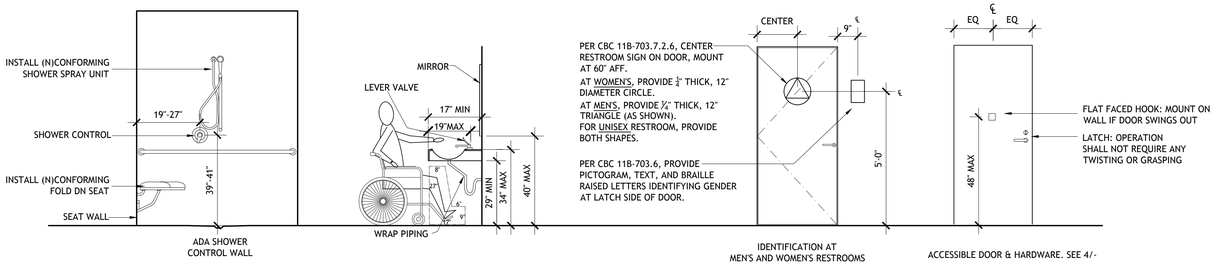
SHEET TITLE:

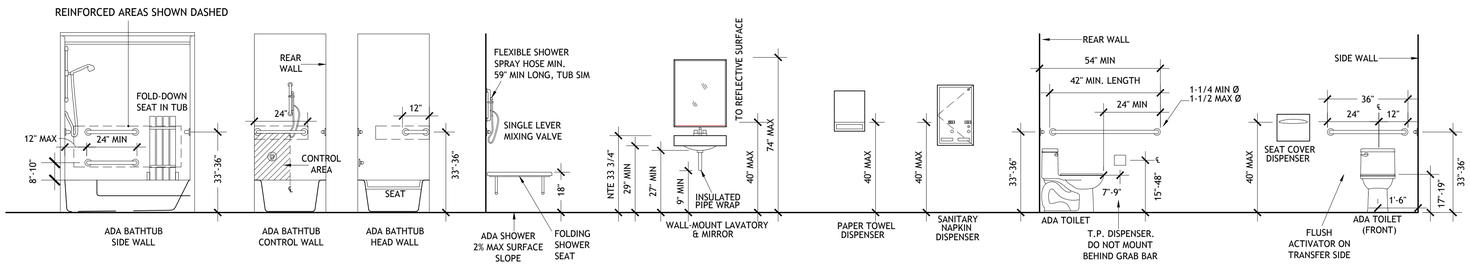
ACCESSIBILITY DETAILS:
EXTERIOR

A5.3

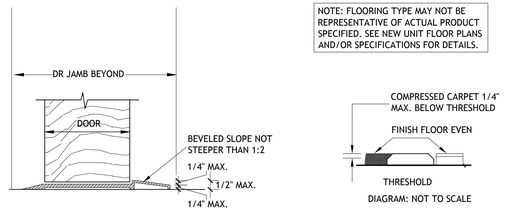
NOTES:
1. GRAB BARS AND TOILET SEAT AND ALL ASSOCIATED FASTENERS SHALL SUSTAIN 250 POUND FORCE IN DIRECT LOAD, POINT LOAD OR SHEAR AT ANY POINT. GRAB BARS SHALL NOT ROTATE WITHIN FITTINGS. INSTALL BLOCKING AS REQUIRED.
2. WALL SURFACES SHALL BE FREE OF SHARP OR ABRASIVE ELEMENTS. EDGES SHALL HAVE A MINIMUM RADIUS OF $\frac{1}{8}$".
3. ALL FAUCETS SHALL BE OF LEVER TYPE AND USABLE WITH ONE HAND TO OPERATE.

PROVIDE SHOWER/TUB WITH SHOWER SPRAY UNIT WITH A HOSE 59" LONG MINIMUM THAT CAN BE USED BOTH AS A FIXED POSITION SHOWER HEAD AND AS A HAND-HELD SHOWER. THE SHOWER SPRAY UNIT SHALL HAVE AN ON/OFF CONTROL WITH A NON-POSITIVE SHUT OFF.



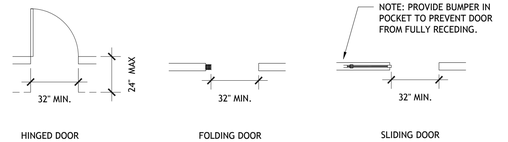


5 REFERENCE MOUNTING HEIGHTS FOR BATHS
SCALE: 3/8" =1'-0"



NOTE: FLOORING TYPE MAY NOT BE REPRESENTATIVE OF ACTUAL PRODUCT SPECIFIED. SEE NEW UNIT FLOOR PLANS AND/OR SPECIFICATIONS FOR DETAILS.

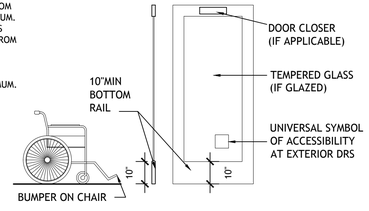
1 THRESHOLD
SCALE: 3"= 1'-0"



NOTE: ADDITIONAL CLEAR FLOOR SPACE MAY BE REQUIRED. COMPLY WITH ADAAG 404.2.4 FOR MANEUVERING CLEARANCES.

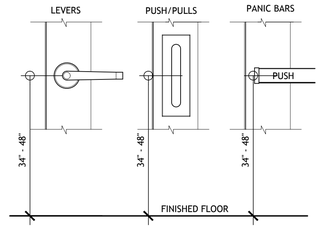
2 MIN. CLEARANCES AT DOORS
SCALE: 1/4" =1'-0"

1. DOOR AND GATE CLOSERS SHALL BE ADJUSTED SO THAT FROM AN OPEN POSITION OF 90 DEGREES, THE TIME REQUIRED TO MOVE THE DOOR POSITION 12 DEGREES FROM THE LATCH IS 5 SECONDS MINIMUM.
2. DOOR AND GATE SPRING HINGES SHALL BE ADJUSTED SO THAT FROM THE OPEN POSITION OF 70 DEGREES, THE DOOR OR GATE SHALL MOVE TO THE CLOSED POSITION IN 1.5 SECONDS MINIMUM.



3 DOOR CLOSER AND KICKPLATE
SCALE: 3/8" =1'-0"

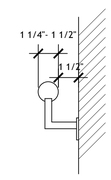
1. OPERABLE PARTS SHALL BE OPERABLE WITH ONE HAND AND SHALL NOT REQUIRE TIGHT GRASPING, PINCHING OR TWISTING OF THE WRIST. THE FORCE REQUIRED TO ACTIVATE OPERABLE PARTS SHALL BE 5LBS MAXIMUM.
2. OPERABLE PARTS OF HARDWARE SHALL BE MOUNTED 34" MIN - 48" MAX ABOVE F.F. OR GROUND. WHERE SLIDING DOORS ARE IN THE FULLY OPEN POSITION, OPERATING HARDWARE SHALL BE EXPOSED AND USABLE FROM BOTH SIDES.



4 DOOR HARDWARE
SCALE: 1 1/2" =1'-0"

NOTES:
HANDRAILS ARE REQUIRED ON PEDESTRIAN RAMPS THAT PROVIDE ACCESS, EXCEPT THAT AT EXTERIOR DOOR LANDINGS, HANDRAILS ARE NOT REQUIRED ON RAMPS LESS THAN 6" RISE OR 72" IN LENGTH.

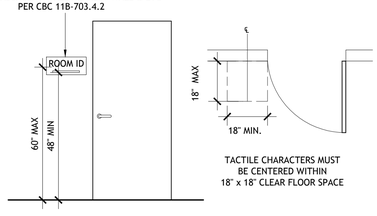
(1) HANDRAILS ARE ON BOTH SIDES AND CONTINUOUS THE FULL LENGTH OF THE RAMP.
(2) FINISH HEIGHT OF HANDRAILS 34"-38" ABOVE RAMP SURFACE.
(3) HANDRAILS EXTEND A MIN. OF 12" OF LEVEL DISTANCE BEYOND TOP AND BOTTOM OF THE RAMP AND ARE RETURNED.
(4) HANDRAIL HAS SMOOTH SURFACE, NO SHARP EDGES OR CORNERS, AND EDGES HAVE A MIN. RADIUS OF $\frac{1}{8}$".
(5) HANDRAIL HAS ENOUGH MIN. STRUCTURAL STRENGTH TO SUPPORT A 250-LB. POINT LOAD.
(6) HANDRAILS DO NOT ROTATE WITHIN THEIR FITTINGS.
(7) WALL OR OTHER SURFACE ADJACENT TO THE HANDRAIL IS FREE OF SHARP OR ABRASIVE ELEMENTS.
(8) ENDS OF HANDRAILS ARE EITHER ROUNDED OR RETURNED SMOOTHLY TO FLOOR, WALL, OR OTHER POST.



ENLARGED DETAIL (NTS)

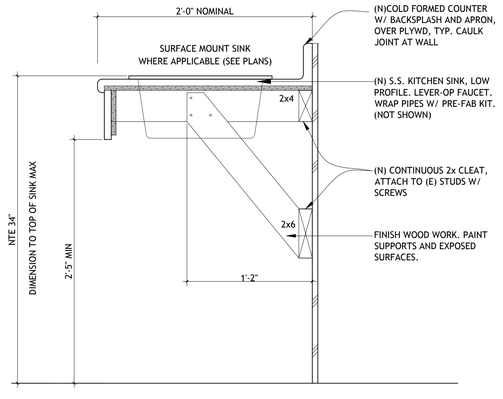
9 HANDRAILS
SCALE: 1 1/2" =1'-0"

ROOM IDENTIFICATION AND OR "EXIT" SIGN, ¾" THICK 12" WIDE W/ BRAILLE RAISED LETTERS, MNT AT LATCH SIDE OR IF NO SPACE AT THE LATCH SIDE, SIGNS SHALL BE LOCATED ON THE NEAREST ADJACENT WALL NEAREST WALL & ETC PER CBC 11B-703.4.2



TACTILE CHARACTERS MUST BE CENTERED WITHIN 18" x 18" CLEAR FLOOR SPACE

7 INTERIOR OR EXTERIOR ROOM I.D.
SCALE: 3/8" =1'-0"



8 LAUNDRY FOLDING COUNTER AND SINK
SCALE: 1 1/2" = 1'-0"

ACCESSIBLE REACH RANGES:

1. A CLEAR FLOOR SPACE (30"x48") SHALL BE PROVIDED FOR EITHER A FORWARD OR PARALLEL APPROACH.
2. FORWARD REACH:
   - 2.1. UNOBSTRUCTED: THE HIGH FORWARD REACH SHALL BE 48 INCHES MAXIMUM AND THE LOW FORWARD REACH SHALL BE 15 INCHES MINIMUM ABOVE THE F.F. OR GROUND.
   - 2.2. OBSTRUCTED HIGH REACH: WHEN REACHING OVER AN OBSTRUCTION, THE CLEAR FLOOR SPACE SHALL EXTEND BENEATH THE ELEMENT FOR A DISTANCE NOT LESS THAN THE REQUIRED REACH DEPTH OVER THE OBSTRUCTION. THE HIGH FORWARD REACH SHALL BE 48 INCHES ABOVE F.F. MAXIMUM WHERE REACH DEPTH IS UP TO 20 INCHES, AND 44 INCHES ABOVE F.F. WHEN REACH DEPTH IS 20-25 INCHES MAXIMUM. NO OBSTRUCTION MAY BE GREATER THAN 25".
3. SIDE REACH:
   - 3.1. UNOBSTRUCTED: THE HIGH SIDE REACH SHALL BE 48 INCHES ABOVE F.F. MAXIMUM AND THE LOW SIDE REACH SHALL BE 15 INCHES MINIMUM ABOVE F.F. OR GROUND.
   - 3.2. OBSTRUCTED HIGH REACH: WHEN THE SIDE REACH IS OVER AN OBSTRUCTION, THE HEIGHT OF THE OBSTRUCTION SHALL BE 34 INCHES MAXIMUM AND THE DEPTH OF THE OBSTRUCTION SHALL BE 24 INCHES MAXIMUM. THE HIGH SIDE REACH SHALL BE 48 INCHES AND THE FOR A REACH DEPTH UP TO 10 INCHES, AND 46 INCHES MAXIMUM FOR A REACH DEPTH OF 10-24 INCHES MAXIMUM.
4. OPERABLE PARTS:
   - 4.1. OPERABLE PARTS SHALL BE WITHIN REACH THE ACCESSIBLE REACH RANGES.
   - 4.2. OPERATIONAL PARTS SHALL HAVE THE REQUIRED CLEAR FLOOR SPACE.
   - 4.3. OPERABLE PARTS SHALL BE OPERABLE WITH ONE HAND AND SHALL NOT REQUIRE TIGHT GRASPING OR TWISTING OF THE WRIST. THE FORCE REQUIRED TO ACTIVATE OPERABLE PARTS SHALL BE 5 POUNDS MAXIMUM.

COMMUNICATIONS FEATURES:

1. NOTIFICATION: A DOOR BELL SHALL BE PROVIDED THAT INDICATES AN AUDIBLE TONE AND VISUAL SIGNAL WITHIN THE UNIT.
2. IDENTIFICATION: A PEEPHOLE WITH A MINIMUM VIEWABLE ANGLE OF 180 DEGREES & MOUNTED AT 43 INCHES ABOVE F.F. SHALL BE INSTALLED AT UNIT ENTRANCE DOORS.

6 ACCESSIBILITY NOTES

BASIS Architecture & Consulting

2130 FOURTH ST
SAN RAFAEL, CA 94901
PHONE (415) 457-6035
FAX (415) 457-6036

P.O.BOX 150539
SAN RAFAEL, CA 94915

CHARLES PICK, ARCHITECT

LICENSED ARCHITECT
CHARLES PICK
C-21406
6-2019
RENEWAL DATE
STATE OF CALIFORNIA

DRAWING REVISION LOG

FOR BIO	5-15-2018

PROJECT NAME:

DESERT HAVEN
(QUEEN'S MOTEL)
RE-DEVELOPMENT

PROJECT LOCATION:

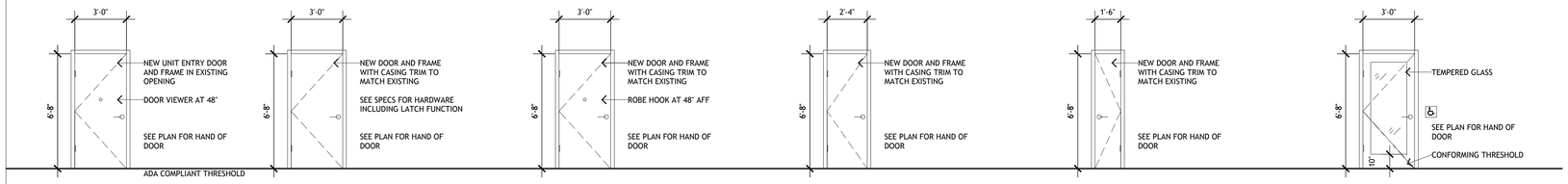
16959 STODDARD WELLS RI
VICTORVILLE, CA 92395

SHEET TITLE:

ACCESSIBILITY DETAILS: INTERIOR

SCALE: AS NOTED

A5.4



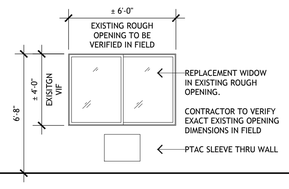
1 EXTERIOR 1 3/4" UNIT ENTRY
2 INTERIOR 1 3/8" DOOR ACCESSIBLE BEDROOM
3 INTERIOR 1 3/8" DOOR ACCESSIBLE BATHROOM
4 1 3/8" INTERIOR CLOSET DOOR
5 1 3/8" INTERIOR CLOSET DOOR
6 NEW ACCESSIBLE EXTERIOR DOOR IN EXISTING OPENING
7 EXTERIOR 1 3/4" INACTIVE STORAGE
8 INTERIOR 1 3/4" DOOR ACCESSIBLE BEDROOM
9 SLIDING CLOSET DOORS
GROUND FLOOR UNIT ENTRY DOORS SIGNAGE EXTERIOR FACE
SECOND FLOOR UNIT ENTRY DOORS SIGNAGE EXTERIOR FACE

DOOR TYPES
SCALE: 1 /4" = 1'-0"

DOOR SCHEDULE

#	LOCATION	DOOR SIZE	DOOR TYPE	HARDWARE FUNCTION	NOTES
1	TYPICAL UNIT ENTRY	3'-0" X 6'-8" X 1 3/4" DOOR AND FRAME IN EXISTING OPENING	HINGED	ENTRY	FURNISH WITH ADA CONFORMING LEVER HARDWARE, THRESHOLD AND DOOR VIEWER
2	TYPICAL UNIT BEDROOM	3'-0" X 6'-8" X 1 3/8" DOOR AND FRAME IN NEW OPENING	HINGED	PASSAGE	FURNISH WITH ADA CONFORMING LEVER HARDWARE
3	TYPICAL UNIT BATHROOM	3'-0" X 6'-8" X 1 3/8" DOOR AND FRAME IN NEW OPENING	HINGED	PASSAGE	FURNISH WITH ADA CONFORMING LEVER HARDWARE, AND ROBE HOOK ON INTERIOR FACE
4	CLOSET	2'-4" X 6'-8" X 1 3/8" DOOR AND FRAME IN NEW OPENING	HINGED	PASSAGE	FURNISH WITH ADA CONFORMING LEVER HARDWARE
5	CLOSET	1'-6" X 6'-8" X 1 3/8" DOOR AND FRAME IN NEW OPENING	HINGED	PASSAGE	FURNISH WITH ADA CONFORMING LEVER HARDWARE
6	COMMUNITY ROOM	3'-0" X 6'-8" X 1 3/4" DOOR AND FRAME IN EXISTING OPENING	HINGED	ENTRY	FURNISH WITH ADA CONFORMING LEVER HARDWARE, THRESHOLD AND DOOR VIEWER
7	TYPICAL UNIT	PAIR 3'-0" X 6'-8" DOORS AND FRAME IN EXISTING OPENING	HINGED	STORAGE	
8	OFFICE UNIT	2'-6" X 6'-8" X 1 3/4" DOOR AND FRAME IN EXISTING OPENING	HINGED	OFFICE	NEW FRAME AND DOOR IN EXISTING ROUGH OPENING (REVERSES SWING OF EXISITGN DOOR TO BE REMOVED OFFICE LOCK FUNCTION WITH DOOR ALWAYS LOCKED ON PUBLIC SIDE
9	CLOSET	5'-0" X 6'-8" CASED GYP BRD OPENING	BI-PARTING	CLOSET	

CONTRACTOR TO VIF ALL SIZES AND REPLACEMENTS



W1 GROUND FLOOR

WINDOW TYPES
SCALE: 1 /4" = 1'-0"

WINDOW DEMOLITION NOTES

1 THE CONTRACTOR SHALL BE RESPONSIBLE FOR REMOVING AND REINSTALLING ALL WINDOW COVERINGS TO ALLOW FOR NEW WORK.
2 THE CONTRACTOR SHALL BE RESPONSIBLE FOR MOVING ALL TENANT'S FURNITURE A MINIMUM OF 5' IN EVERY DIRECTION FROM THE WORK AREA
3 THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROJECTION OF THE EXISTING FINISHES AND THE TENANT'S FURNISHINGS.
4 THE CONTRACTOR SHALL BE RESPONSIBLE FOR CLEANING REFUSE REMOVAL AND THE END OF EACH WORK DAY.

WINDOW NOTES - CONTRACTOR TO NOTIFY ARCHITECT OF ANYDISCREPANCIES PRIOR TO WINDOW ORDER

1 IN ADDITION TO REQUIRED MEANS OF EGRESS ALL EXISTING SLEEPING ROOMS HAVE ONE EMERGENCY EGRESS WINDOW WITH A MINIMUM CLEAR OPENING OF 5.7 SQUARE FEET (EXCEPT THE MINIMUM NET CLEAR OPENING FOR EMERGENCY ESCAPE AND RESCUE GRADE FLOOR OPENINGS COULD BE 5 SQUARE FEET).
THE MINIMUM NET CLEAR OPENING DIMENSIONS ARE GREATER THAN 24 INCHES (H) AND 20 INCHES (W). THE NET CLEAR OPENING DIMENSIONS SHALL BE THE RESULT OF NORMAL OPERATION OF THE OPENING.
THE MAXIMUM HEIGHT FROM THE FLOOR FOR EMERGENCY EGRESS WINDOWS IS NOT GREATER THAN 44 INCHES.
2 ALL WINDOWS IN THE DWELLING UNITS ABOVE THOSE AT GRADE SHALL BE EQUIPPED WITH A CHILDPROOF LATCH THAT WILL NOT ALLOW THE WINDOW TO BE OPENED MORE THAN 4 INCHES.
3 THE CONTRACTOR SHALL BE RESPONSIBLE FOR VERIFYING ALL EXISTING ROUGH OPENINGS DIMENSIONS FOR NEW REPLACEMENT WINDOWS IN EXISTING OPENINGS AND MAKE ALL NECESSARY ADJUSTMENTS FOR THE FIELD VERIFIED DIMENSIONS IN THE SHOP DRAWING SUBMITTAL REQUIRED FOR REPLACEMENT WINDOWS.
4 WINDOW GLAZING IN HAZARDOUS LOCATIONS REQUIRE SAFETY GLAZING MATERIALS, EITHER TEMPERED GLASS OR LAMINATED SAFETY GLASS. WIRED GLASS IS NOT PERMITTED.
5 CONTRACTOR PRIOR TO WINDOW ORDER TO PROVIDE FIXED PANEL LOCATIONS AS REQ'D FOR CONFORMING VENTING CLEARANCES AT RELOCATED GAS METERS & NEW HOT WATER HEATER LOCATIONS.

GLAZING NOTES

1 THE FOLLOWING ARE CONSIDERED HAZARDOUS LOCATIONS REQUIRING SAFETY GLAZING:
GLAZING IN AN INDIVIDUAL FIXED OR OPERABLE PANEL ADJACENT TO A DOOR WHERE THE NEAREST EXPOSED EDGE OF THE GLAZING IS WITHIN A 24 INCH ARC OF EITHER VERTICAL EDGE OF THE DOOR IN A CLOSED POSITION AND WHERE THE BOTTOM EXPOSED EDGE OF THE GLAZING IS LESS THAN 60 INCHES ABOVE THE WALKING SURFACE.

WINDOW SCHEDULE

* WINDOWS DIMENSIONS CONTRACTOR TO VIF	OPERATION	GLAZING	LOCATION	NOTES
W1 REPLACEMENT WINDOW IN ± 6'-0" X 4'-0" EXISTING OPENING	FLIXED/SLIDER	DUAL GLAZED WITH LOW E GLASS	TYPICAL UNIT	CONTRACTOR TO VERIFY EXACT ROUGH OPENINGS IN FILED AND ACCOUNT FOR THE DIMENSIONS IN S SHOP DRAWING SUBMITTALL FOR REVIEW AND APPROVAAL PRIOR TO ORDERING

NOTES:

CONTRACTOR TO VIF ALL SIZES & REPLACEMENTS
(R) = REPLACE EXISTING
(N) = NEW

THE OPERABLE PORTION OF THE WINDOWS IS TO BE PROTECTED WITH INSECT SCREENS

BASIS Architecture & Consulting

2130 FOURTH ST
SAN RAFAEL, CA 94901
PHONE (415) 457-6035
FAX (415) 457-6036

P.O.BOX 150539
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LICENSED ARCHITECT
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WINDOW AND DOOR SCHEDULES

A6.0